



2036 ST LEONARDS + CROWS NEST

# 270 PACIFIC HIGHWAY

CROWS NEST NSW 2065

270 PACIFIC HIGHWAY PLANNING PROPOSAL - Rev. 02

20 APRIL 2022

fitzpatrick+partners

SITE ANALYSIS

03	LOCATION
04	EXISTING CONTEXT + CURRENT DEVELOPMENTS
05	ORIENTATION + TOPOGRAPHY
06	TREE CANOPY + VISTAS
07	FINE GRAIN + STREETScape
08	HERITAGE + CONSERVATION

CONTROLS ANALYSIS

10	ZONING + HERITAGE
11	MOVEMENT + LANDSCAPE
12	HEIGHT OF BUILDINGS
13	FSR + STREET WALL HEIGHT
14	SETBACKS + SOLAR HEIGHT PLAN

PLANNING STUDY

16	BASEMENT FLOOR PLANS
17	LOWER GROUND + GROUND FLOOR PLANS
18	PODIUM FLOOR PLANS
19	TYPICAL TOWER FLOOR PLANS
20	TOP OF TOWER FLOOR PLANS
21	CROSS SECTION
22	CROSS SECTION
23	LONG SECTION
24	EAST ELEVATION
25	WEST ELEVATION
26	NORTH ELEVATION
27	SOUTH ELEVATION

SHADOWS STUDY

29	9:00 - 10:00 WINTER SOLSTICE SHADOWS
30	10:30 - 1:00 WINTER SOLSTICE SHADOWS
31	1:30 - 3:00 WINTER SOLSTICE SHADOWS

STREETScape VIEWS STUDY

33	STREETScape VIEWS I
34	STREETScape VIEWS II
35	STREETScape VIEWS III

SCHEDULE OF AREAS

37	SCHEDULE OF AREAS
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The site lies within the 2036 St Leonards and Crows Nest plan. This area is being densified and developed into a vibrant community, employment hub, greener and accessible place while protecting its past.

- + Located opposite the Five Ways special precinct
- + 400m from Mater hospital and health and education precinct
- + 400m from future Crows Nest metro



- MAJOR ROAD
- NORTH SHORE TRAIN LINE
- METRO LINE
- SITE
- PROJECT NORTH





**1. EMBASSY TOWER (BUILT)**  
29-STORY TOWER; FIRST HIGH RISE IN THE AREA BEFORE OTHER DEVELOPMENTS PROPOSED



**2. 88 BY JQZ (DA APPROVED)**  
3 HIGH RISE TOWER DEVELOPMENT NEXT TO ST LEONARDS TRAIN STATION; TWO RESIDENTIAL TOWERS (26-STORIES AND 47-STORIES) AND ONE 14-STORY COMMERCIAL TOWER



**3. THE LANDMARK (UNDER CONSTRUCTION)**  
44-STORY TOWER+EQUALLY TALL TOWERS ARE BEING CONSTRUCTED IN THE CENTRE-STATION



**4. ST LEONARDS SQUARE (UNDER CONSTRUCTION)**  
TWO RESIDENTIAL TOWERS (27-STORIES AND 35-STORIES)



**5. GATEWAY TO THE PRECINCT**  
THE SITE+FIVE WAYS INTERSECTION FORM THE GATEWAY TO THE PRECINCT FROM THE SOUTH

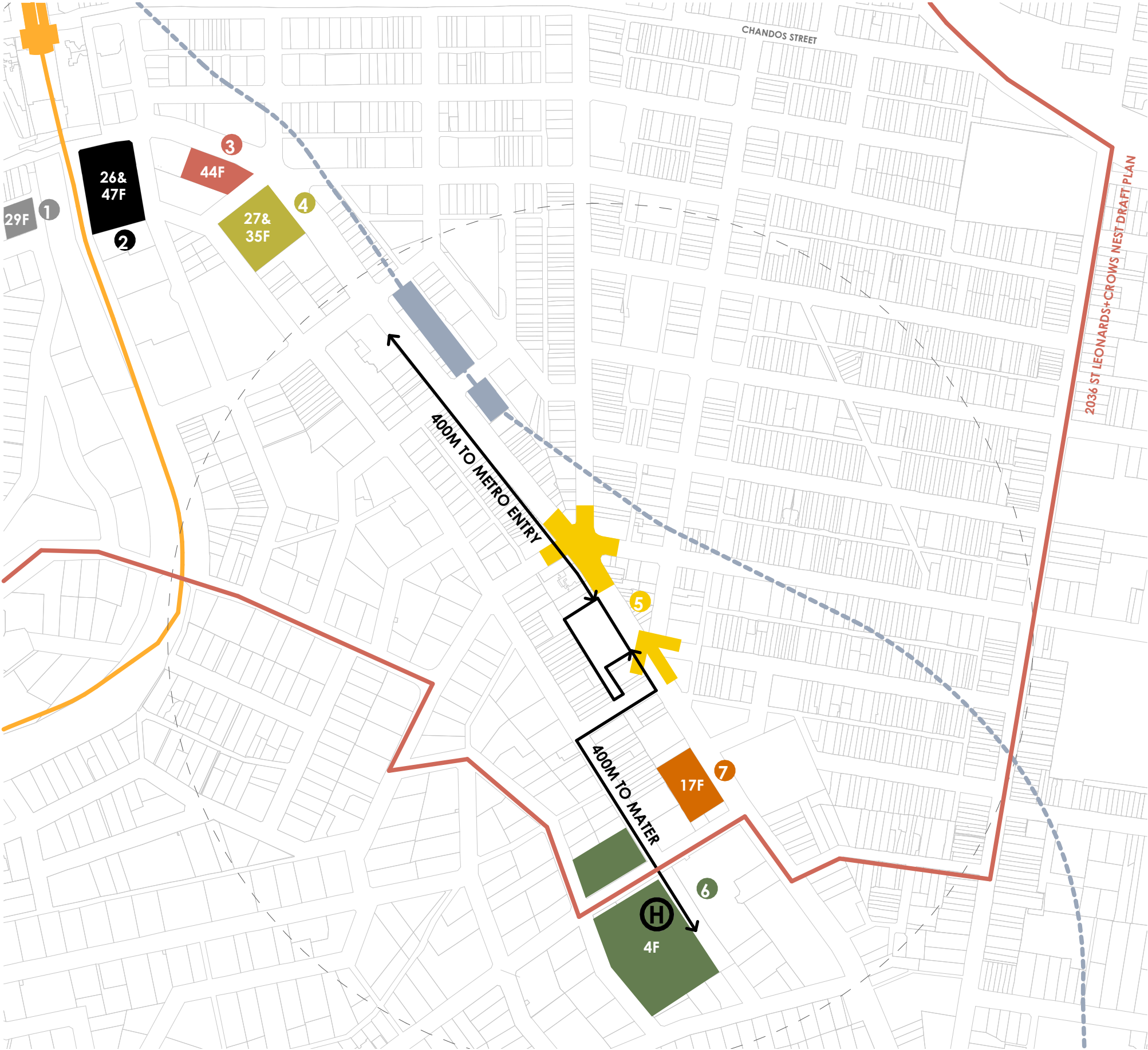


**6. MATER HOSPITAL (BUILT)**  
THE SITE CAN CATER TO THE EXISTING HEALTH DISTRICT DIRECTLY SOUTH (4 STOREYS)



**7. 200-220 PACIFIC HIGHWAY (BUILT)**  
17 STOREY RESIDENTIAL TOWER

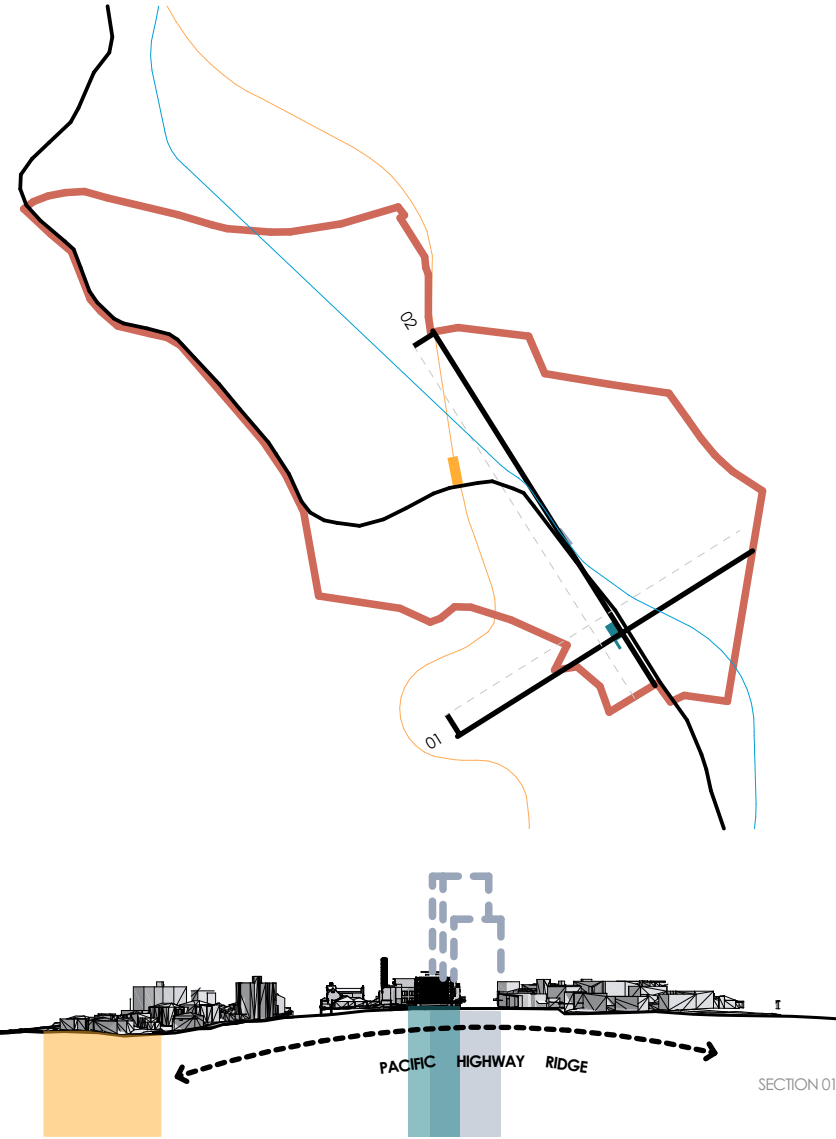
The site is unique in that it is surrounded by heritage listed sites such as the crows nest hotel and fire station but still has been declared a precinct for densification and high rise towers.



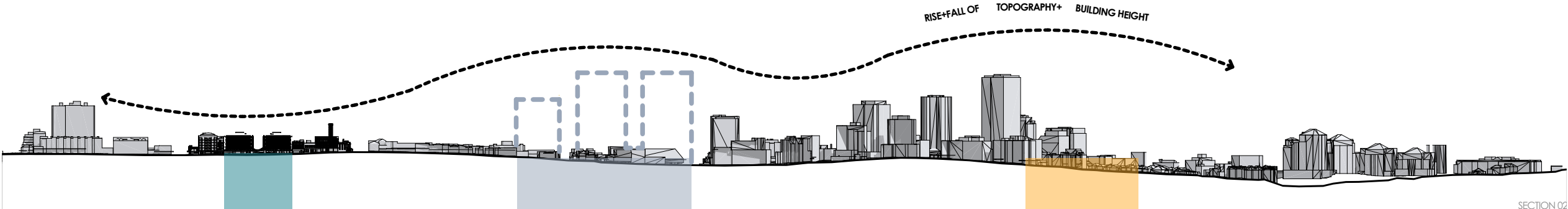


The site is receptive to the seasonal winds from the south east and south west with access to sunlight due to its topographical location.

- + Site with a northeast/southwest orientation
- + Long east and west exposed facades will require sun shading
- + North and south facades are blocked by existing developments up to podium level
- + Potential overshadowing to residential properties to the west



- ST LEONARDS STATION
- METRO STATION
- SITE
- PROJECT NORTH







**1. WILLOUGHBY+BURLINGTON**  
ASYMMETRICAL PLANTING+ MATURE TREES BLOCK  
POTENTIAL VIEWS OF THE SITE



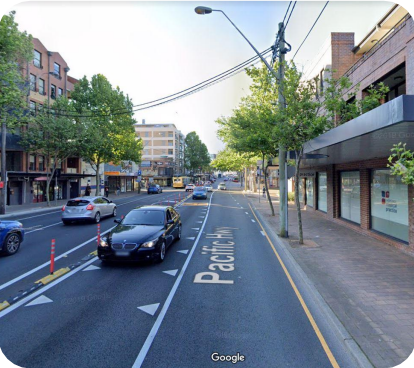
**2. FALCON+ALEXANDER**  
OPPORTUNITY TO SEE SITE IS DIMINISHED BY LEAFY  
ROAD+WOLWORTHS



**3. FALCON+ALEXANDER**  
LESS TREE CANOPY ALLOWS MORE OPPORTUNITY FOR  
VIEWING OF THE SITE FROM AFAR



**4. HAYBERRY**  
DENSE TREE CANOPY AT STREET PARK ALLOWS VERY  
LOW VISIBILITY OF SITE



**5. PACIFIC HIGHWAY**  
MAIN APPROACH TO THE SITE ALLOWS OPTIMUM VISIBILITY  
FROM SIDE-ON



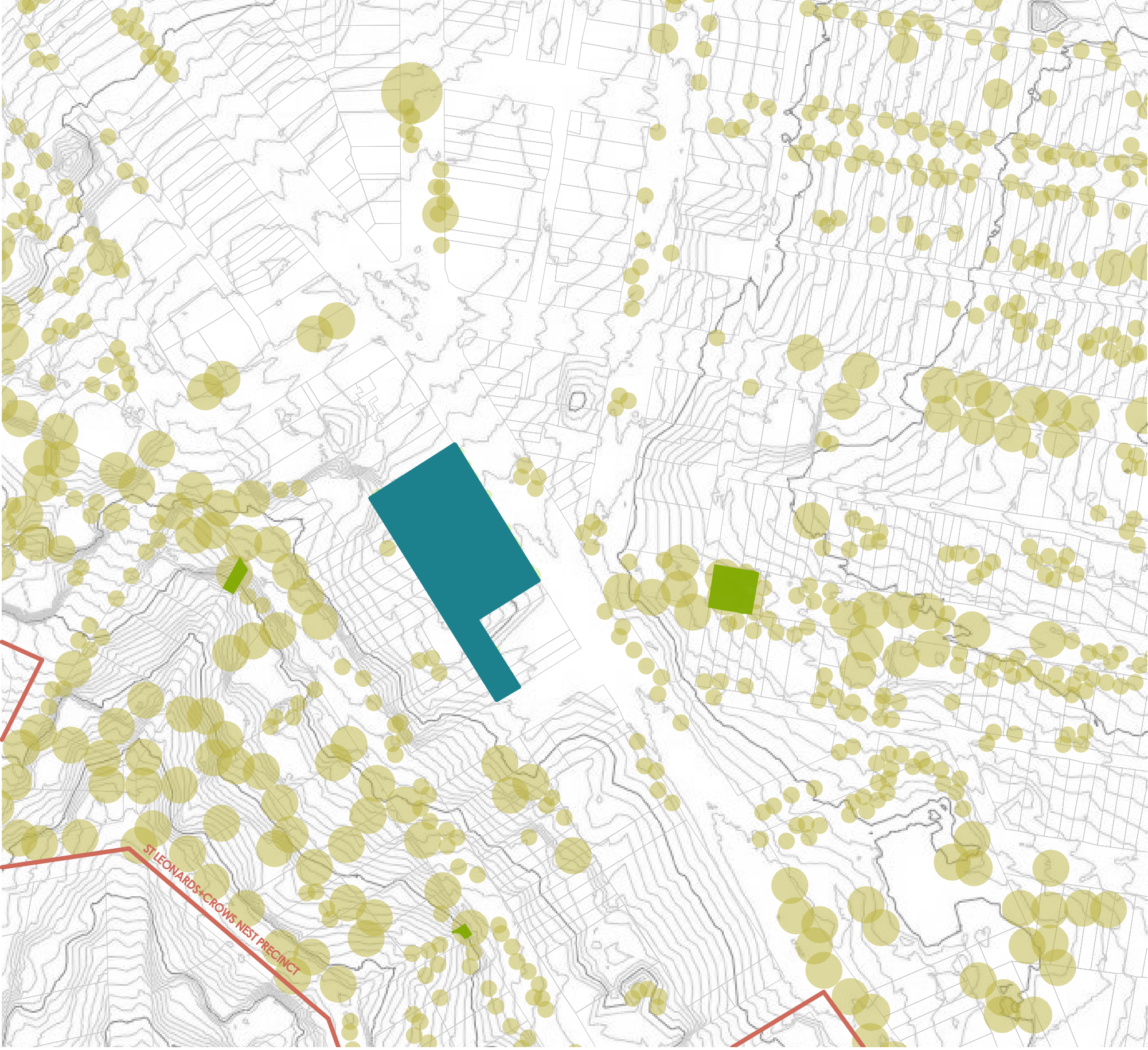
**6. SINCLAIR+BRUCE**  
TOPOGRAPHY OF THE SITE ALONG THE PACIFIC  
HIGWAY RIDGE SHOULD ALLOW BETTER VISIBILITY



**7. SINCLAIR+SHIRLEY**  
TREE CANOPY CLEARS AROUND THE HERITAGE LISTED  
BUILDINGS AND IMPORTANCE IS PLACED ON SEEING  
THE HERITAGE FACADES

The street tree planting is sporadic but generally mature in surrounding streets, in particular, Willoughby Road, leading to reduced distant vistas toward the site.

- PUBLIC OPEN SPACE (RE1 ZONE)
- TREE CANOPY
- SITE
- PROJECT NORTH



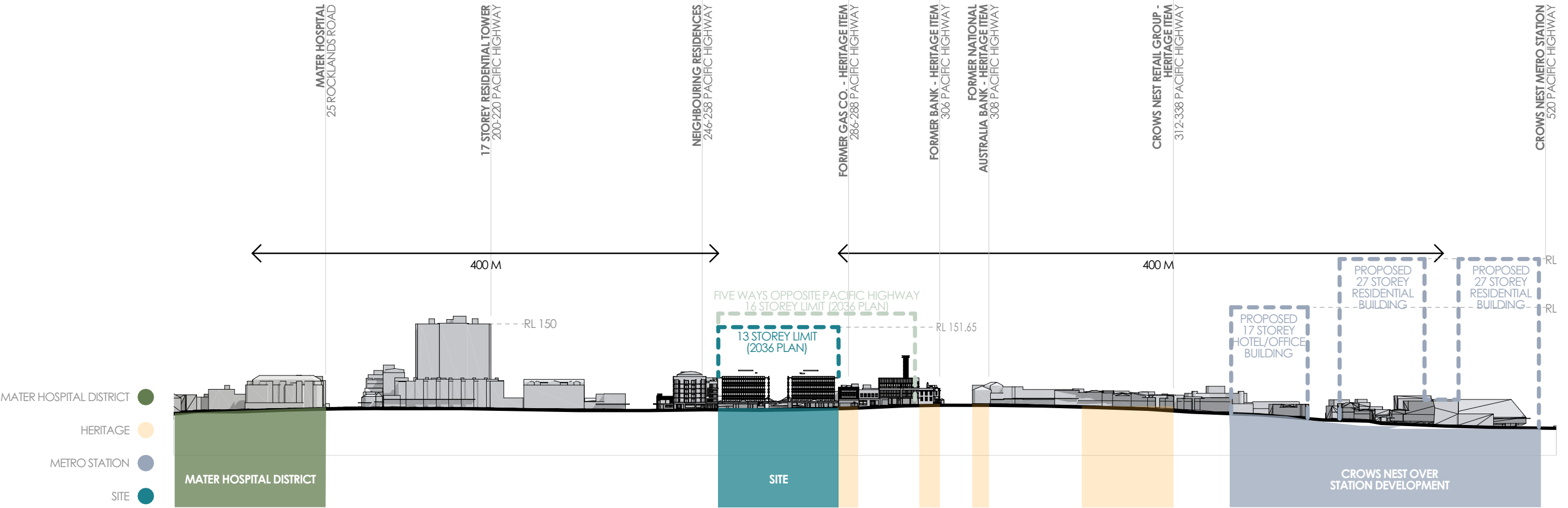




MODULATION OF RETAIL STREETSCAPE

CROWS NEST HOTEL - HERITAGE

FINE GRAIN





**3. CROWS NEST RETAIL GROUP**  
RARE SURVIVING BUILDINGS IN INTERWAR FUNCTIONALIST  
STYLE+ ART DECO DETAILING; FEDERATION FREE STYLE WITH  
UNUSUAL ART NOUVEAU DECOR (ITEM NOS. 10153-10164)



**6. FORMER NATIONAL AUSTRALIA BANK**  
PROMINENT CORNER SITE+EARLIEST BUILDING OF THE  
COHESIVE INTERWAR COMMERCIAL (ITEM NO. 10152)



**7. FORMER BANK**  
PROMINENT STREETSCAPE ITEM+INTERWAR REVIVAL OF  
GEORGIAN ARCHITECTURE (ITEM NO. 10151)



**9. CROWS NEST FIRE STATION**  
ARTS+CRRAFTS STYLE IN PUBLIC UTILITY; ONE OF LAST  
STATIONS DESIGNED BY GOVERNMENT ARCHITECT (ITEM  
NO. 10173)

The site is surrounded by over 15  
heritage listed entities of the  
interwar revival period.

The alignment of the building's  
podium with adjoining heritage  
should describe the built form as  
prescribed by the 2036 Plan.

+ Former north shore gas co. to the  
north (2 storeys)

+ Street wall to match height of  
neighbouring heritage buildings

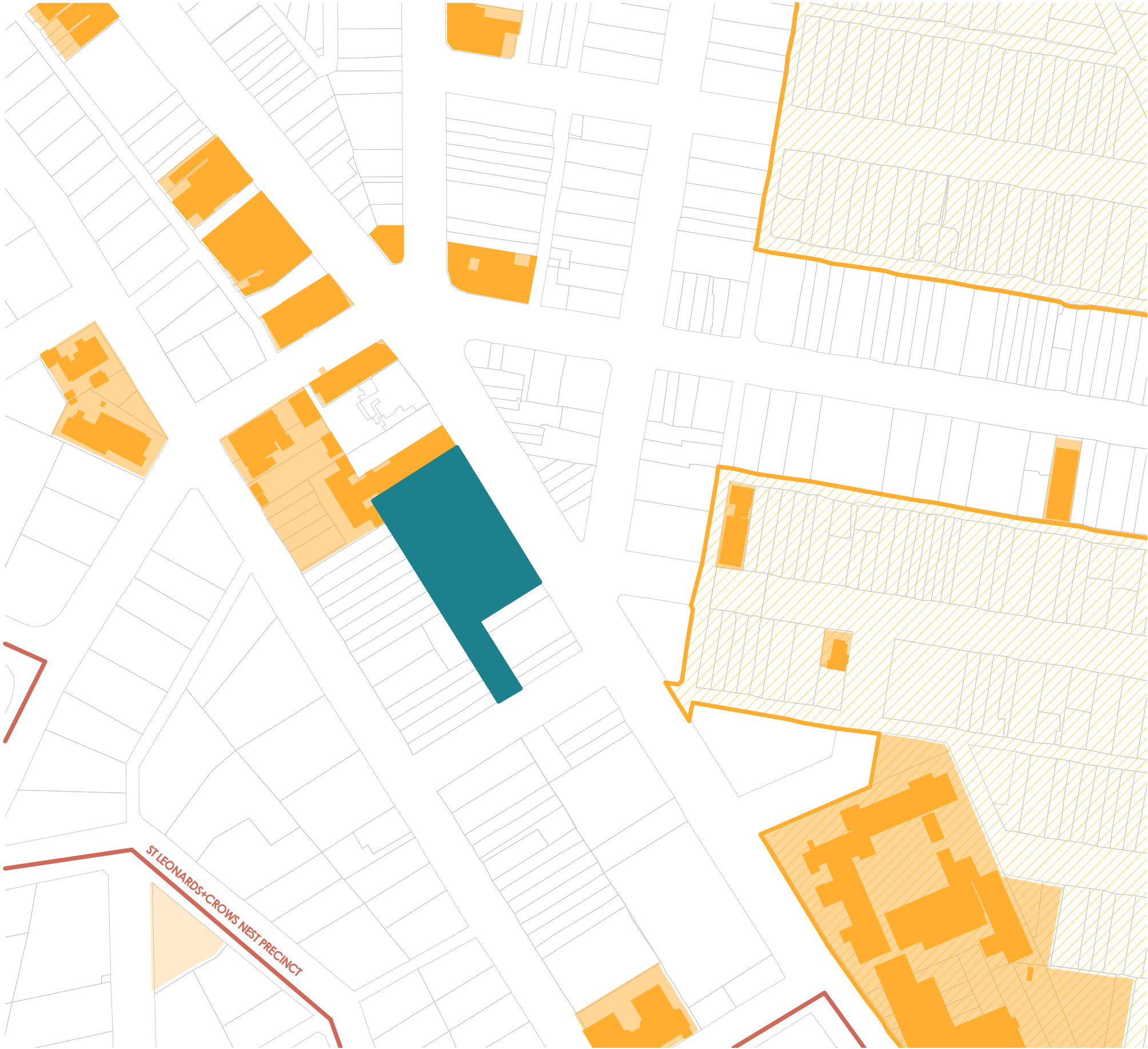
- PODIUM TO MEET STATION  
DESIGN REQUIREMENTS
- 2 STOREY STREET WALL
- 3 STOREY STREET WALL
- 4 STOREY STREET WALL
- STREET WALL TO MATCH  
NEIGHBOURING HERITAGE  
SITE
- HERITAGE LISTED SITE
- CONSERVATION AREA
- SITE
- PROJECT  
NORTH



**10. FORMER GAS CO.**  
TRADITIONAL RETAIL STREETSCAPE: ART DECO SHOP WITH  
SCALLOPED FACADE IN GLAZED TERRACOTTA (ITEM NO.  
10151)



**14. HOUSE**  
ITEM NO. 11088







LAND ZONING

The site is located in a Mixed Use Zone (B4) along Pacific Highway and in close proximity of the heart and vibrancy of Crows Nest village.

The site has a total area of 3,793 m<sup>2</sup> in single ownership and it is located between the future Crows Nest metro station to the north and the existing health and education precinct to the south, making it a prime site to promote employment growth in the Crows Nest and Mater Hospital area as intended in the 2036 Plan.

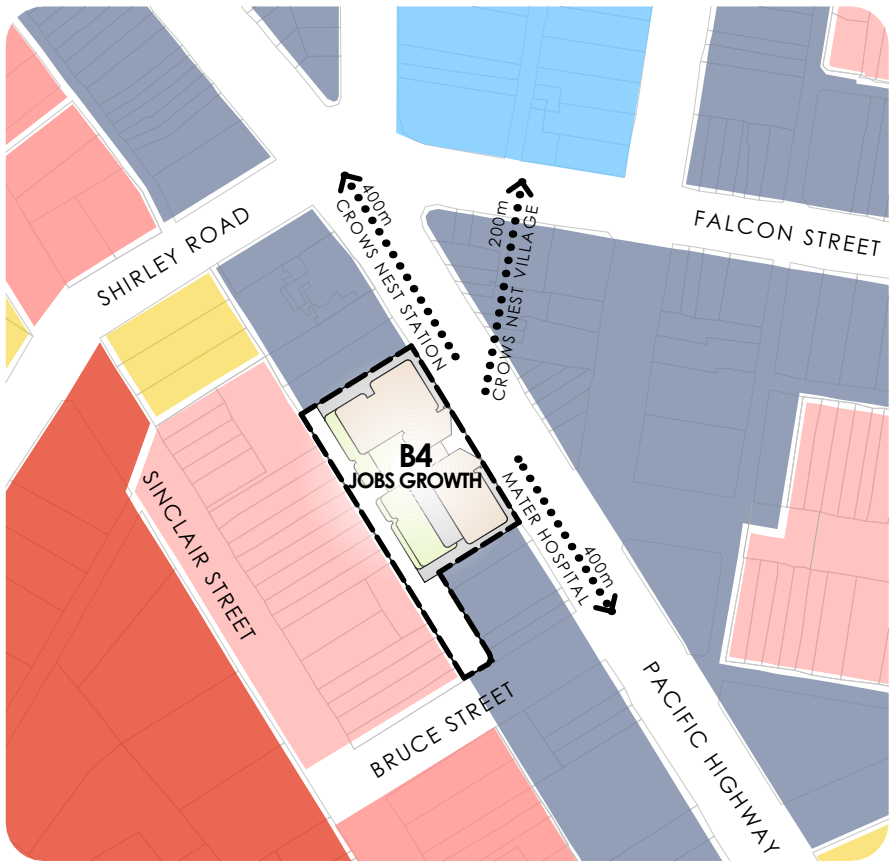
The 2036 Plan aims for 1950 to 3020 new jobs in the Crows Nest area and a further 700 to 1440 in the Mater Hospital, of which some can be supported by adjacent sites along the Pacific Highway corridor.

OUTCOME

Commercial and health related uses to promote employment growth in Crows nest area as envisaged by the 2036 Plan.

COMPLIES ✓

- B1: NEIGHBOURHOOD CENTRE
- B4: MIXED USE
- B3: COMMERCIAL CORE
- R2: LOW DENSITY RESIDENTIAL
- R3: MEDIUM DENSITY RESIDENTIAL
- R4: HIGH DENSITY RESIDENTIAL
- SP2: INFRASTRUCTURE
- RE1: PUBLIC RECREATION
- 2036 DRAFT PLAN ZONING CHANGE AREA



HERITAGE+CONSERVATION

The site is in close proximity of various heritage listed items and conservation areas. To the east, at the corner of Emmett and Alexander lanes, is the edge of the Holtermann Estate C. To the north the Former Gas Co adjoins the site and in the corner of Shirley Road and Pacific Highway is the Former National Australia Bank.

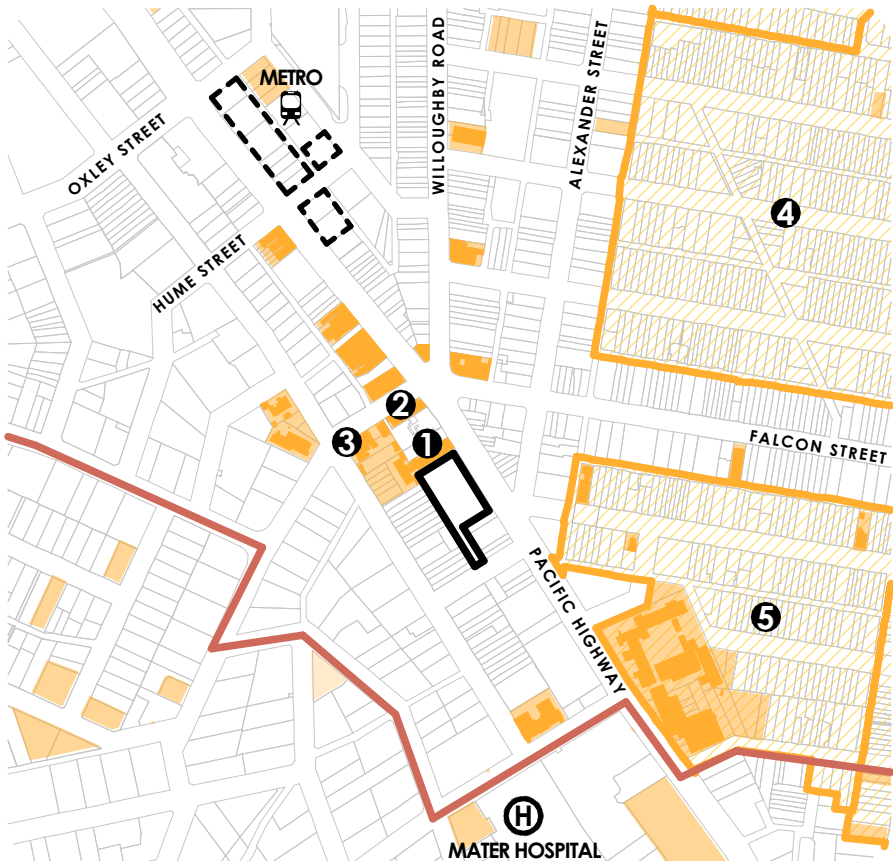
The proposed development will consider the surrounding heritage fabric, in particular, those adjoining the site to ensure a sensitive built form that addresses the existing street height wall and fine grain.

OUTCOME

Podium to be defined by the heritage street height alignment and fine grain.

COMPLIES ✓

- FORMER NORTH SHORE GAS COMPANY 1
- FORMER NATIONAL AUSTRALIAN BANK 2
- CROWS NEST FIRE STATION 3
- HOLTERMANN ESTATE B 4
- HOLTERMANN ESTATE C 5
- HERITAGE LISTED SITE
- CONSERVATION AREA



2036 PLAN

PROPOSED



MOVEMENT

The site is well serviced by public transport being located on Pacific Highway and 400m from the future Crows Nest metro station.

Existing pedestrian and bike routes are proposed to be improved in the 2036 Plan with better links across Pacific Highway and new shared zones along Pacific Highway to the new station.

To take advantage and complement the vision of the draft plan, the new development will consider an articulated street frontage to improve pedestrian movement along the site and will provide a new end of trip facility to promote the use of the bike as a mode of transportation. The new development will also consider reducing the existing parking rate to reduce dependency on private transport.

OUTCOME

*Articulated street frontage to improve pedestrian safety, Reduce existing control's parking rates to minimise private transport dependency, and Provide new EOT to promote bike use and public transport.*

COMPLIES✓

LANDSCAPE

The site is in close proximity of three landscaped pockets classified as RE1 (public recreation). Two are located west of the site on Sinclair Street and one is on the east on Hayberry Street.

Existing tree planting along Pacific Highway and the site is scattered but the draft plan envisages improving the existing conditions.

The new development will respect its surroundings by creating a built form that won't cast extra shadows over any of the existing RE1 zones between the hours of 10am and 3pm.

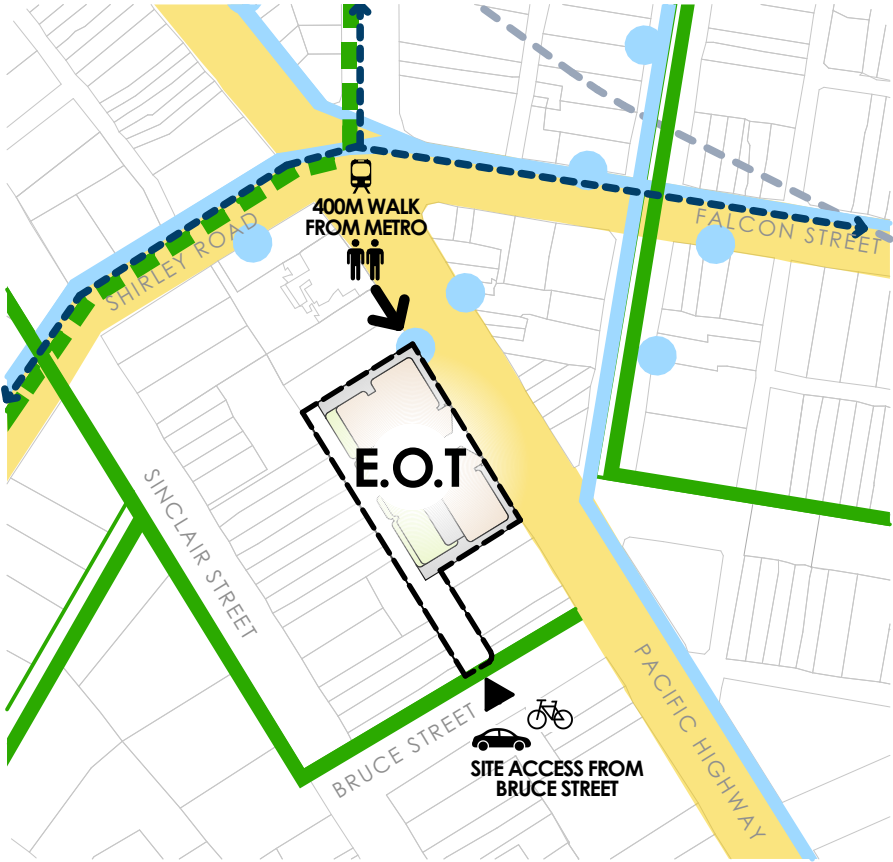
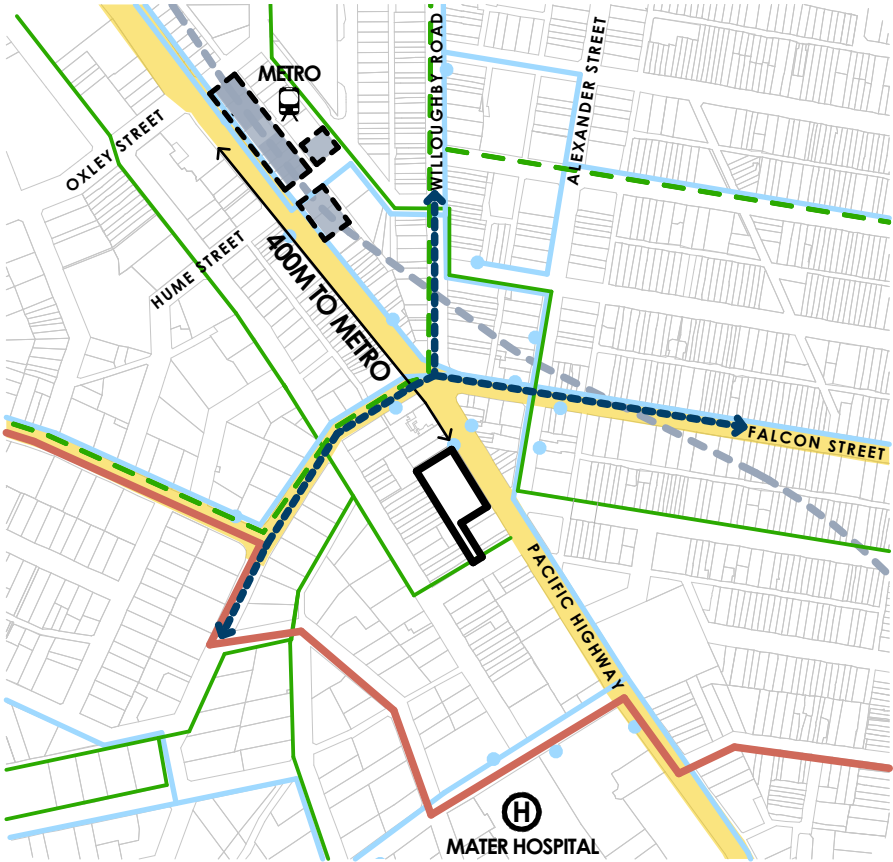
To improve the overall amenity of the area, the new development will provide a public accessible landscaped podium roof top that will provide a protected environment from the busy Pacific Highway.

OUTCOME

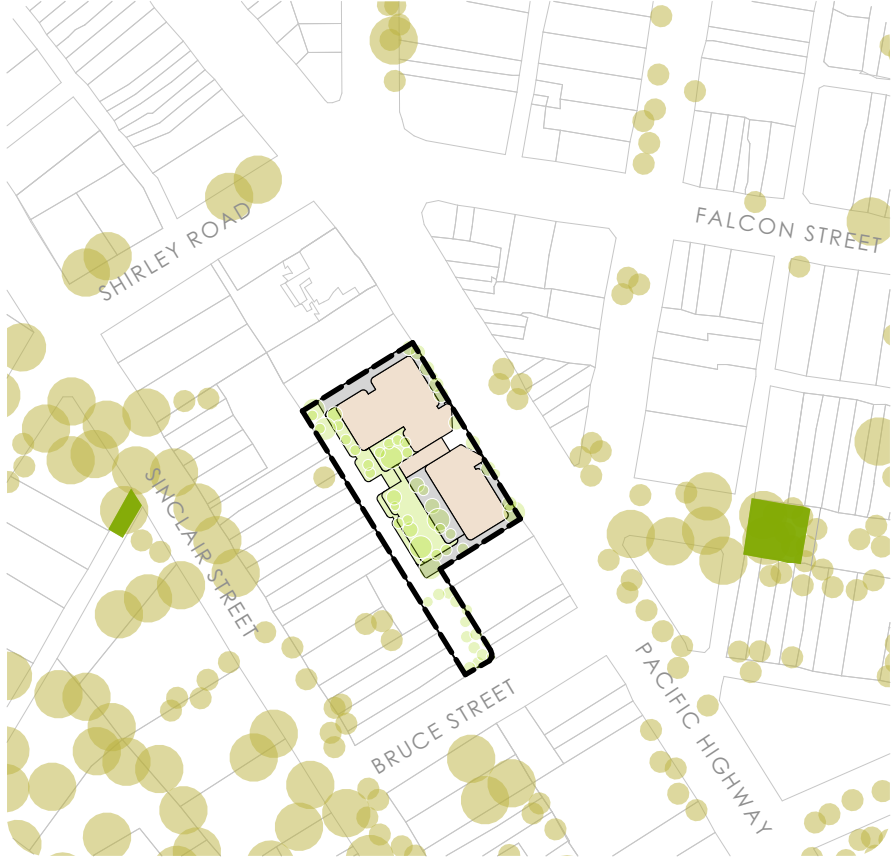
*Retain and or improve street planting along Pacific Highway, No overshadowing of adjacent RE1 zones, and Landscaped podium roof will improve amenity and provide a public accessible space for the wider community.*

COMPLIES✓

- EXISTING PEDESTRIAN LINK
- NEW OR IMPROVED CYCLE INFRASTRUCTURE
- EXISTING CYCLE INFRASTRUCTURE
- BUS STOP
- BUS ROUTE
- MAIN ROAD
- METRO LINE



- PROPOSED OPEN SPACE
- RE1 ZONE
- TREE LINED GREEN STREET
- EXISTING TREE CANOPY
- PROJECT NORTH



2036 PLAN

PROPOSED

HEIGHT OF BUILDINGS

The 2036 plan envisages two clusters of high density where the new station will be located and a transition in height away from Pacific Highway and towards low density areas.

Based on the 2036 Plan, the site has a height of 13 storeys and is in close proximity of 17 to 27 storeys buildings to the north (metro station) and a 17 storey building to the south (220 Pacific Highway). Directly east is the Five Ways site identified as 16 storeys.

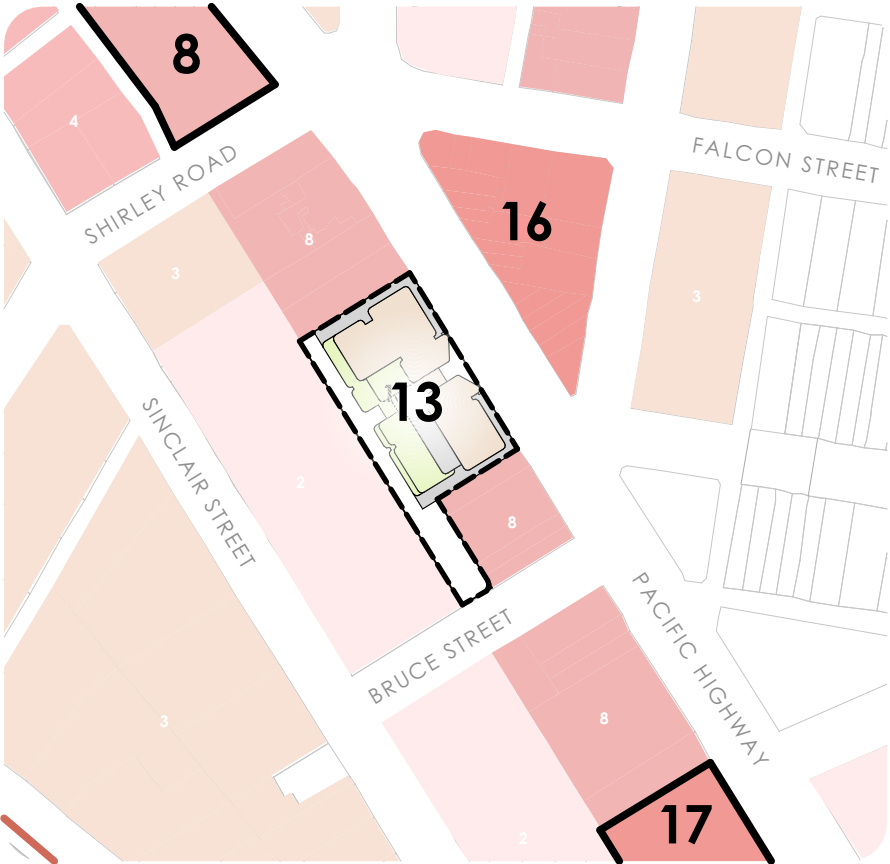
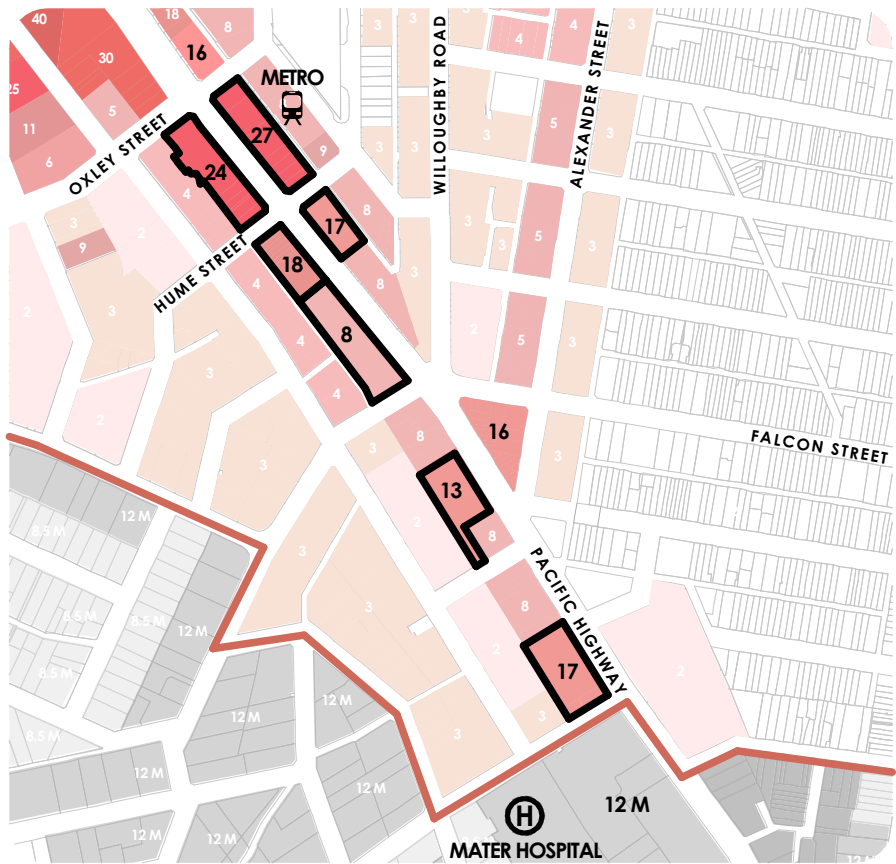
The 2036 plan vision of transitioning the height away from the Crows Nest Metro Station is compromised by existing conditions (17 storey building at the southern end of the precinct). Considering the above and the site's unique conditions (size, location and land use), we believe a building of similar scale to the tall buildings in the precinct could be accommodated along the Pacific Highway ridge line and opposite the significant site of Five Ways.

A taller building that meets view sharing requirements can be accommodated in the site without overshadowing residential areas outside the precinct's boundary, heritage conservation areas, and RE1 zones.

OUTCOME

*Proposed 10 "full" storeys + 3 "terraced" storeys is respectful of the solar access goals, aligns with the 2036 Plan intentions, and creates a built form that better responds to future and existing conditions, including the future Five Ways height to create a Gateway to the Precinct.*

COMPLIES ✓

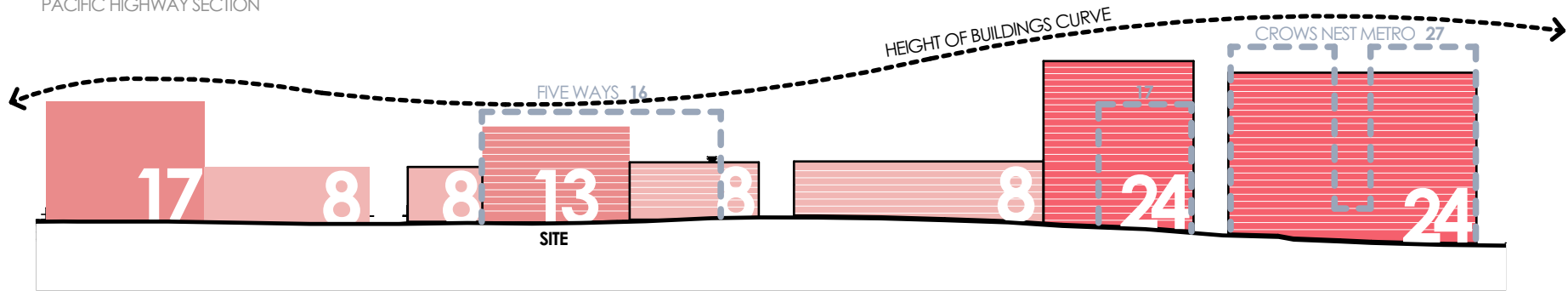


PROJECT  
NORTH

2036 PLAN

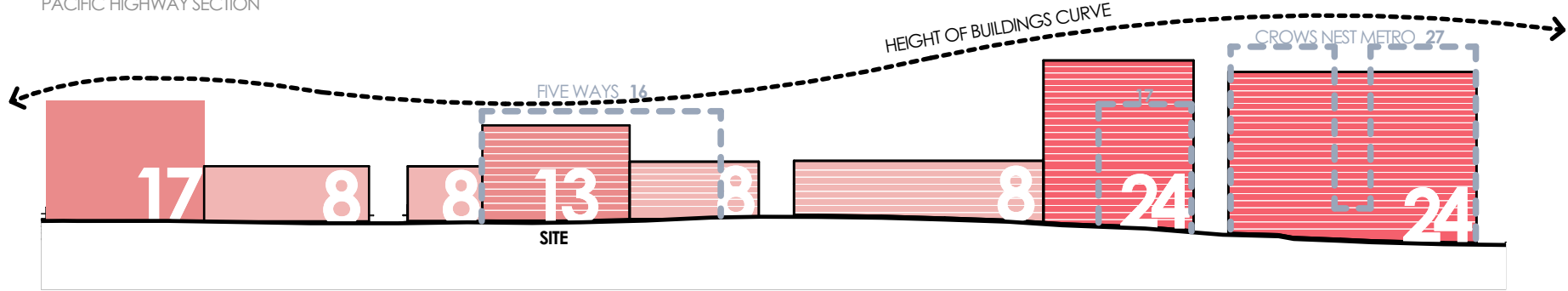
PROPOSED

PACIFIC HIGHWAY SECTION



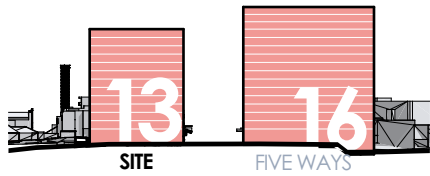
2036 PLAN

PACIFIC HIGHWAY SECTION



PROPOSED

PACIFIC HIGHWAY CROSS SECTION



PACIFIC HIGHWAY CROSS SECTION





FSR

The 2036 Plan proposes an FSR of 5.6:1 with minimum non-residential FSR of 5.6, locking the site's potential future development. Directly opposite the site is the five ways precinct with an FSR of 5.8 and in close proximity, there are sites with FSR's that vary between 6.5:1 and 11.5:1.

Considering the site's close proximity to the future Crows Nest station, the Five Ways "Special Precinct" and Mater Hospital, it's important that the planning controls allow the site to reach it's full development potential. The site is one of the biggest singled-own in the area, well serviced by road infrastructure and public transport, conveniently located close to the existing education and health facilities which will be attractive to new workers on site, and will contribute to the employment growth in the area as envisioned by the plan.

The proposed 6.02:1 FSR is made of the compliant 5.6:1 for the component above ground and 0.6:1 for the area in the lower ground making the future redevelopment of the site feasible and would satisfy the overshadowing and view sharing requirement. The new FSR would result in a built form that is consistent with the vision of the 2036 Plan for the Pacific Highway streetscape and specifically, the buildings located around the new metro station.

OUTCOME

*Proposed 6.02:1 FSR to create a built form that is in keeping with the density and employment growth envisaged for the area by the 2036 Plan.*

STREET WALL HEIGHTS

The 2036 Plan identifies the site's street wall height as 3 which matches the neighbouring heritage street wall height.

The site is adjoined to the north by a 2 storey heritage-listed Former North Shore Gas Co. (equivalent to a 3 storey podium) and to the south by a residential building with a 4 storey street wall height. The proposed development aims to address the existing conditions by creating an articulated podium that respects the scale and fine grain of the existing heritage listed item.

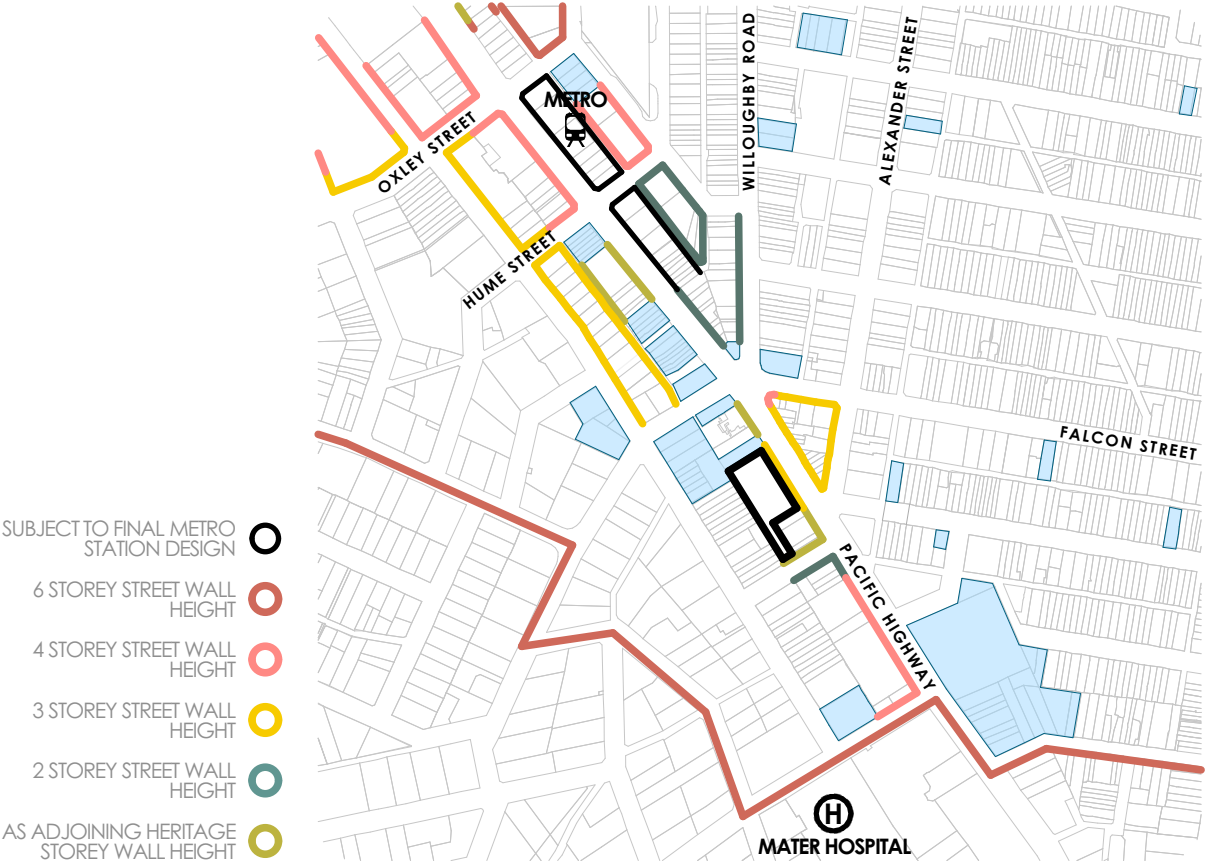
OUTCOME

*Articulated podium that addresses the heritage street wall height to the north.*

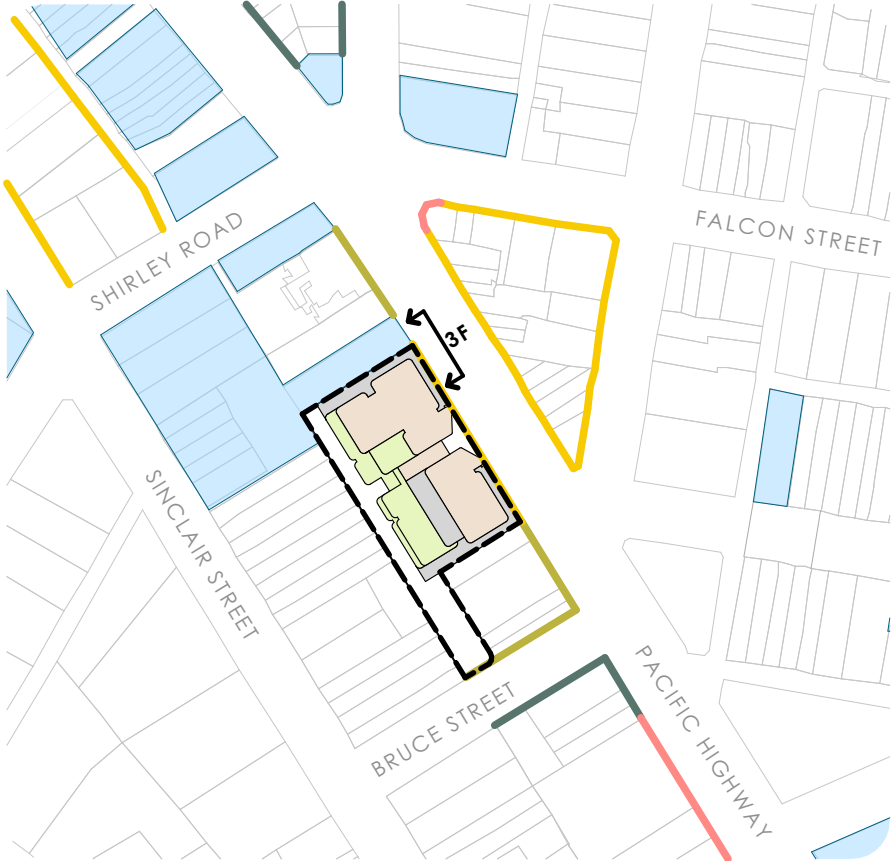
COMPLIES ✓



CROWS NEST METRO SITES ○  
2036 PLAN FSR CHANGES ○



SUBJECT TO FINAL METRO STATION DESIGN ○  
6 STOREY STREET WALL HEIGHT ○  
4 STOREY STREET WALL HEIGHT ○  
3 STOREY STREET WALL HEIGHT ○  
2 STOREY STREET WALL HEIGHT ○  
AS ADJOINING HERITAGE STOREY WALL HEIGHT ○  
PROJECT NORTH ↑



PROPOSED



STREET SETBACKS

The site is identified in the 2036 Plan's Built Form Street Setback map as having a 0m street setback facing Pacific Highway.

The proposed development responds to the control by creating a podium and tower built to the street boundary (Pacific Highway - 0m setback). The two components are separated by a recessed floor set by the heritage street height of the Former Northshore Gas Co. creating a shadow gap between the two volumes.

The podium is proposed to be built to the boundary on 3 sides except facing west where a 6m setback is proposed to maintain a level of privacy and amenity to the existing neighbouring properties. The tower takes a similar approach, but with a 3 & 5m setback to the north and south and a 8 & 10m to the west with terraced top floors to respond to the solar height plane controls.

OUTCOME

Podium - 0m street setback to Pacific Highway and 6m to the rear

Tower - 3m street setback to Pacific Highway, 3m on sides, and 6m to the rear

COMPLIES✓

SOLAR HEIGHT PLANE

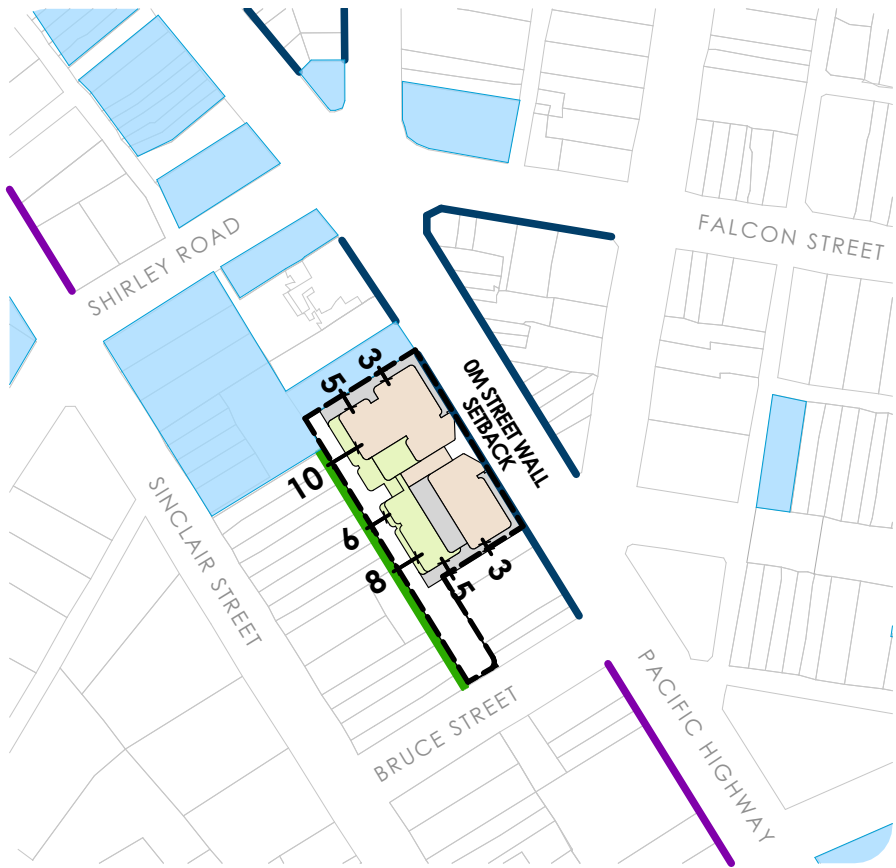
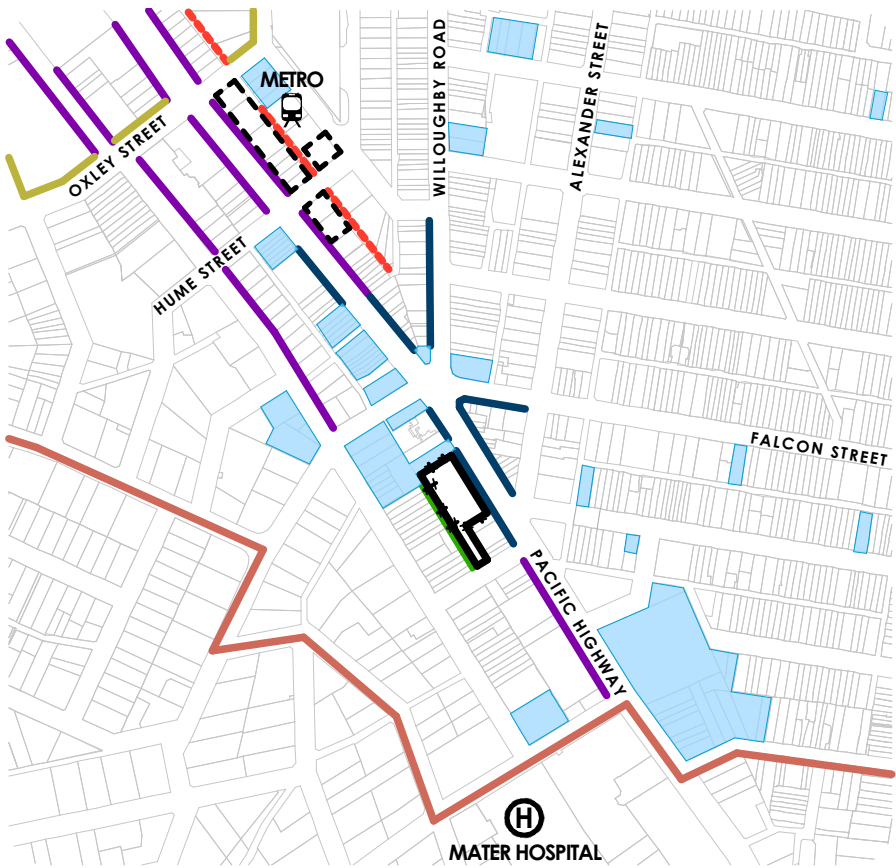
The site is in proximity of a couple of smaller Public Open Spaces (RE1) along Sinclair Street, the precinct's Outside Boundary to the south and west, and the Conservation Area (Holtermann Estate C) to the southeast.

The proposed massing considered the solar access constraints and resulted in an articulated volume that is terraced to the west to avoid overshadowing the residential areas outside the boundary and has its highest point to the north to avoid overshadowing the conservation areas. Due to the site's location, there is no possible overshadowing of any of the protected streetscapes and public open spaces.

OUTCOME

No overshadowing of residential inside Conservation Areas, Outside Boundary, protected Streetscapes and Public Open Spaces.

COMPLIES✓

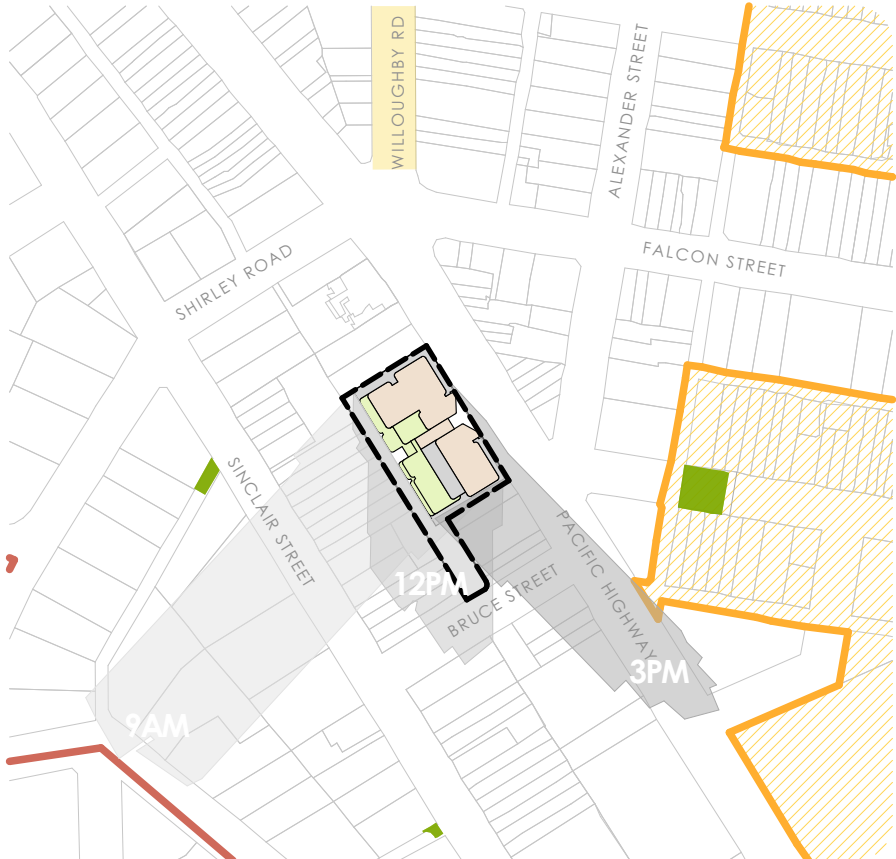
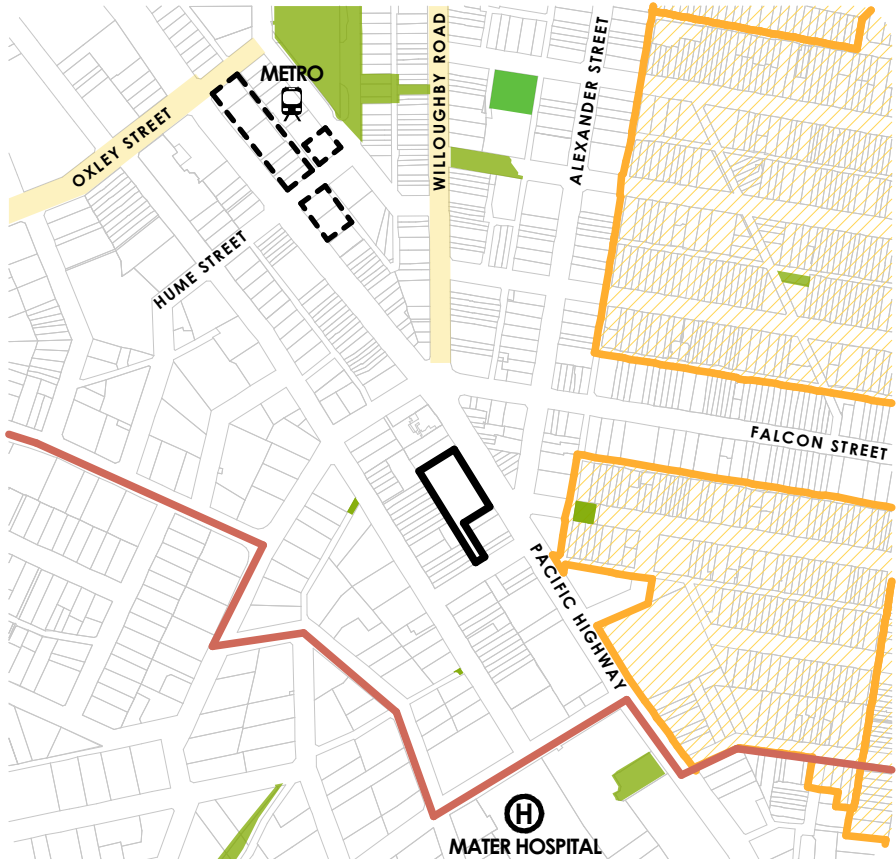


**SOLAR ACCESS**  
Public Open Space 10am to 3pm  
Christie Park, Newlands Park, St Leonards South, Propsting Park, Hume Street Park, Ernest Place, Gore Hill Oval and Talus Reserve

Streetscape 11:30am to 2:30pm  
Mitchell Street, Oxley Street and Willoughby Road

Residential areas 9am to 3pm  
Residential areas inside boundary (for at least 2 hours), Heritage Conservation areas inside boundary (for at least 3 hours) and Residential outside boundary (for the whole time between 9am and 3pm)

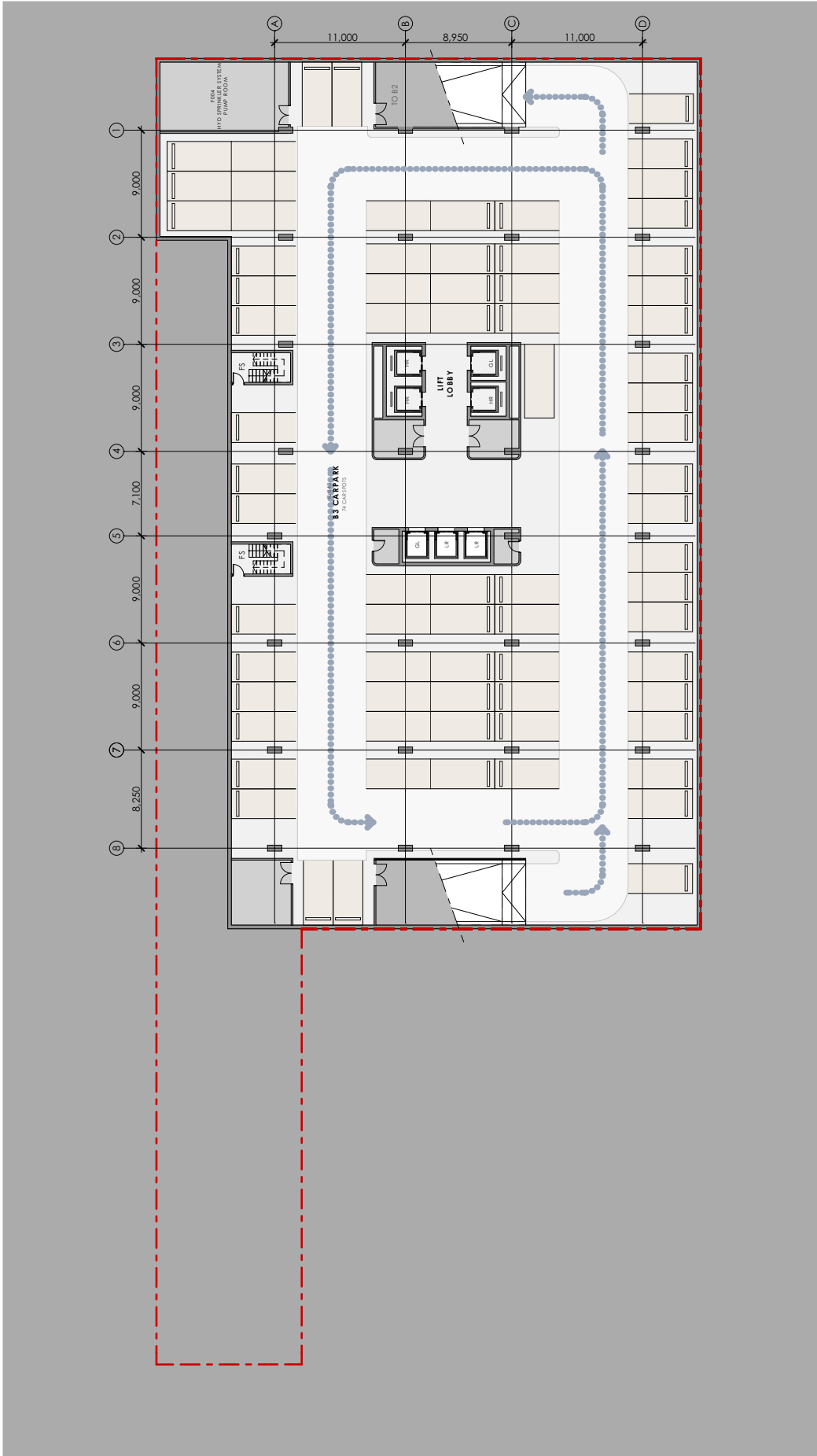
POTENTIAL OPEN SPACE  
RE1 ZONE  
CONSERVATION AREA



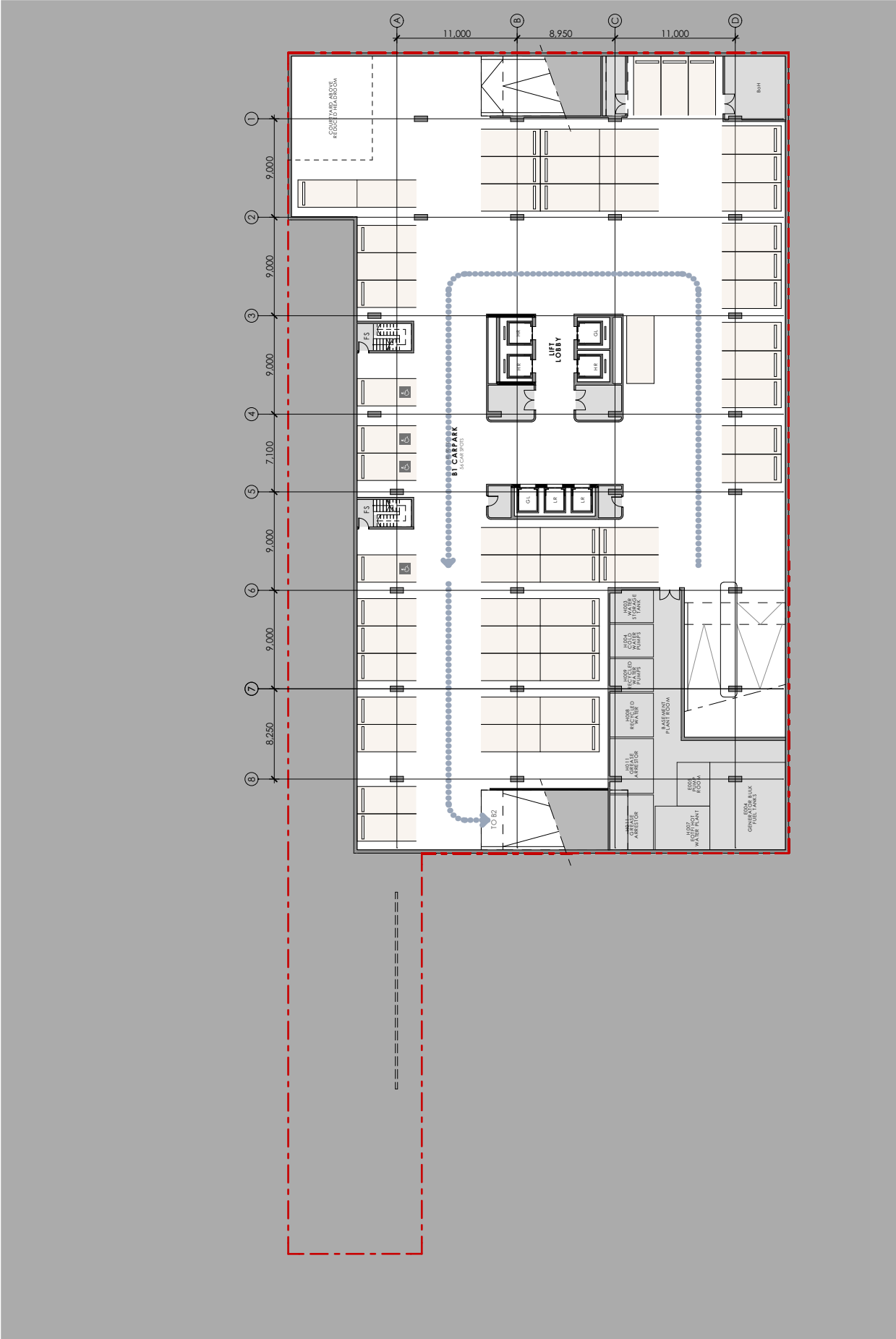
2036 PLAN

PROPOSED

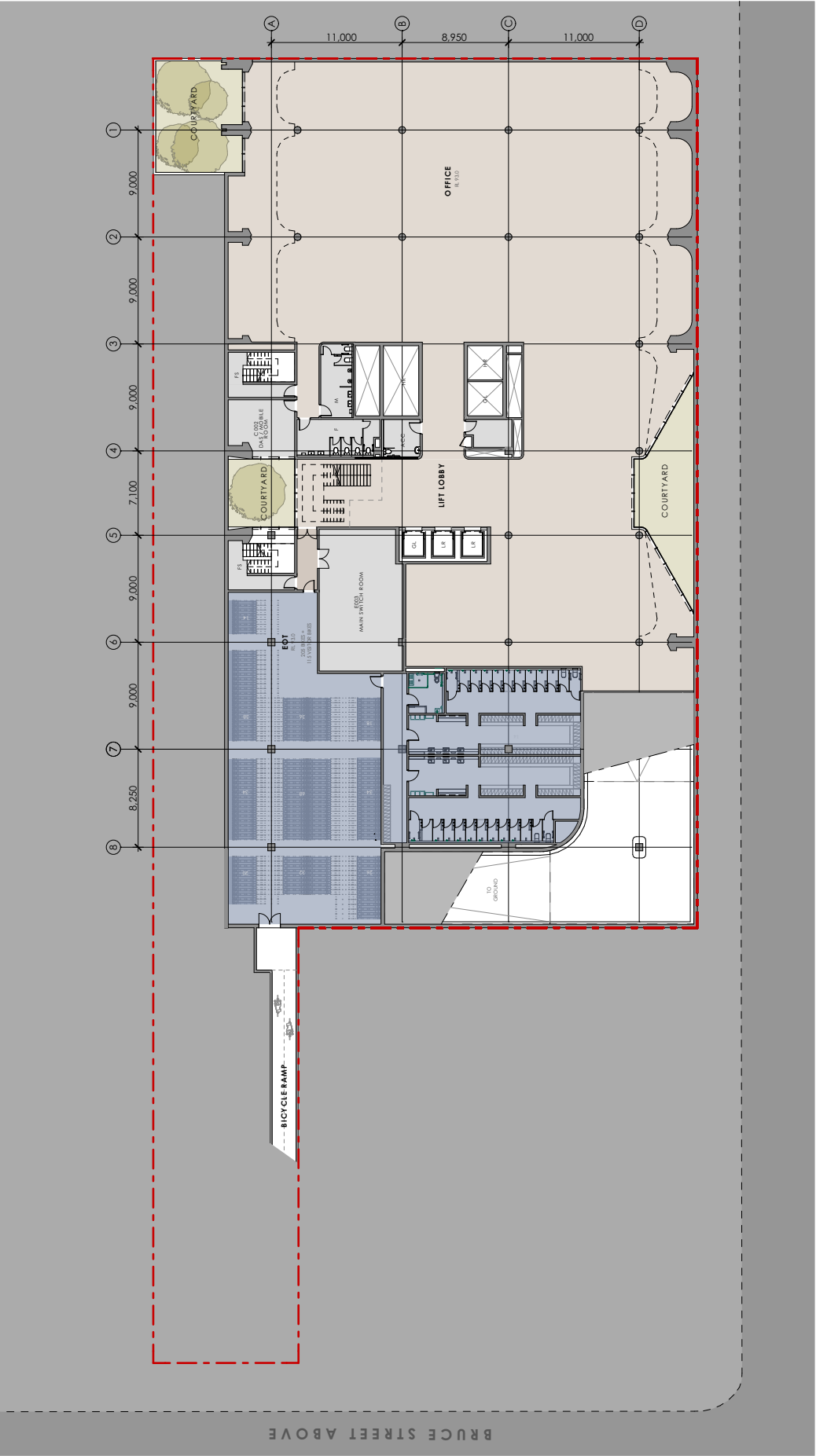




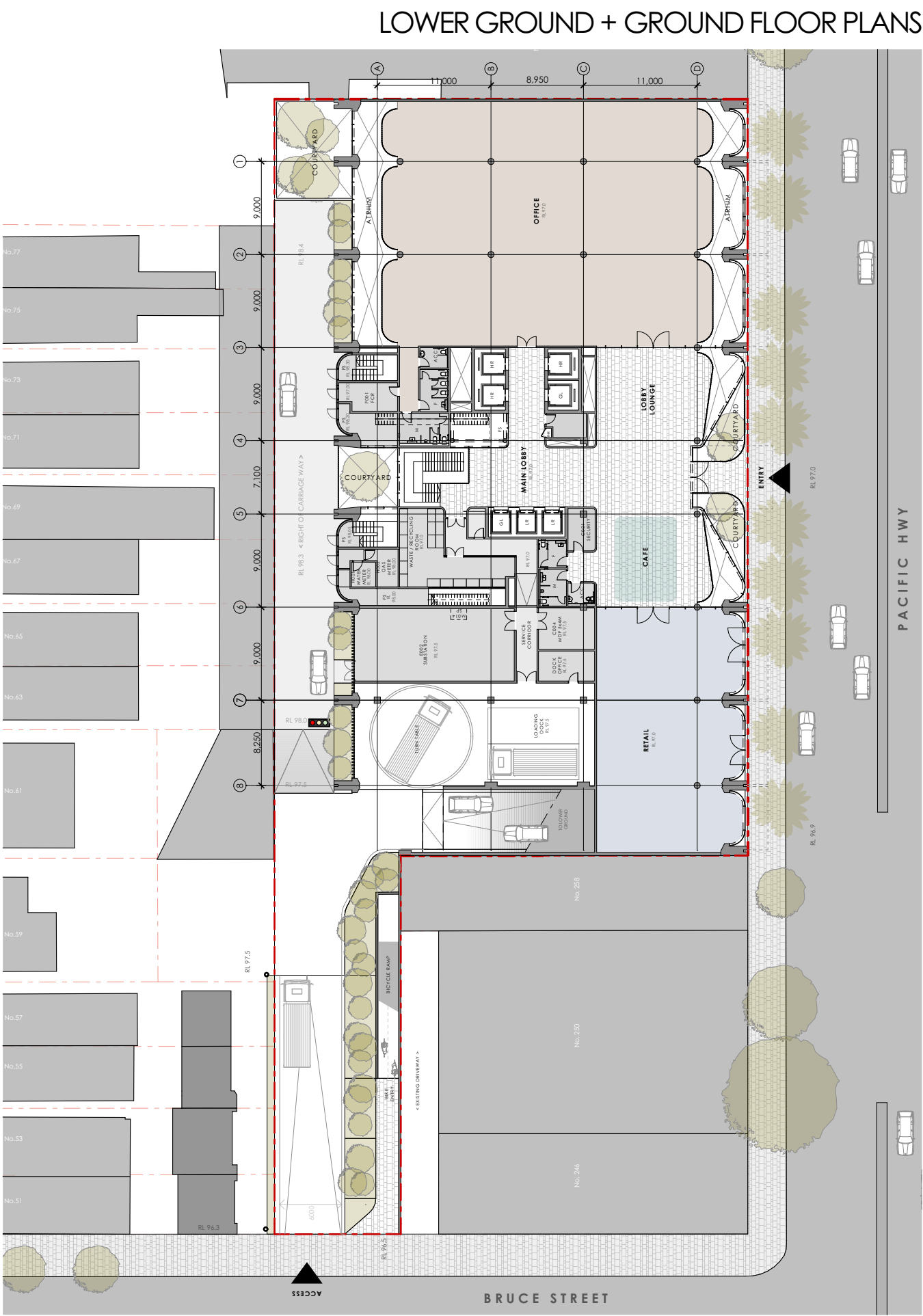
BASEMENT 03+02  
Parking for 74 cars on B3 and 72 cars on B2



BASEMENT 01  
Parking for 56 cars

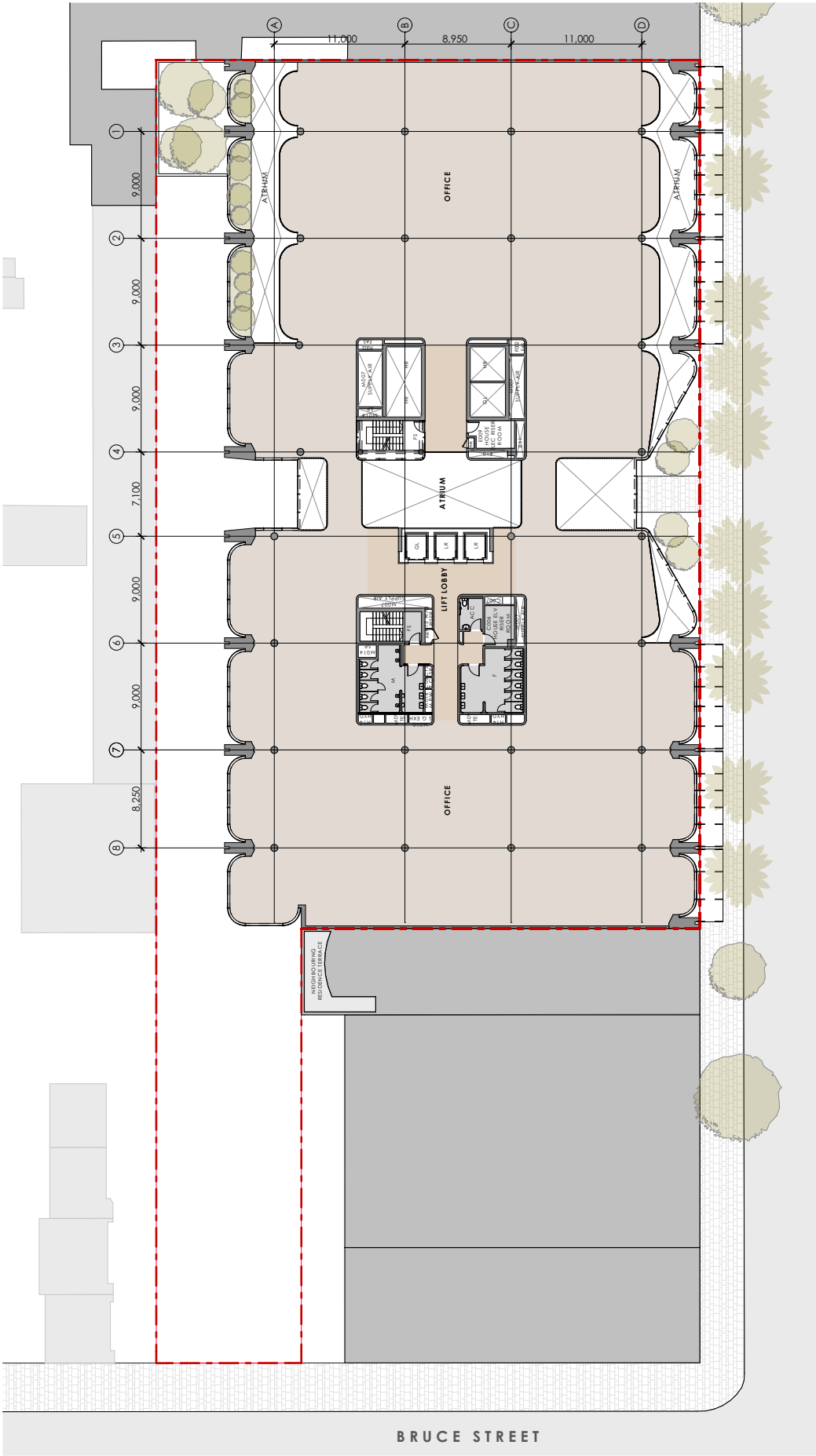


LOWER GROUND  
End of Trip Facilities, Tenant Space, GFA: 1,505 M<sup>2</sup>

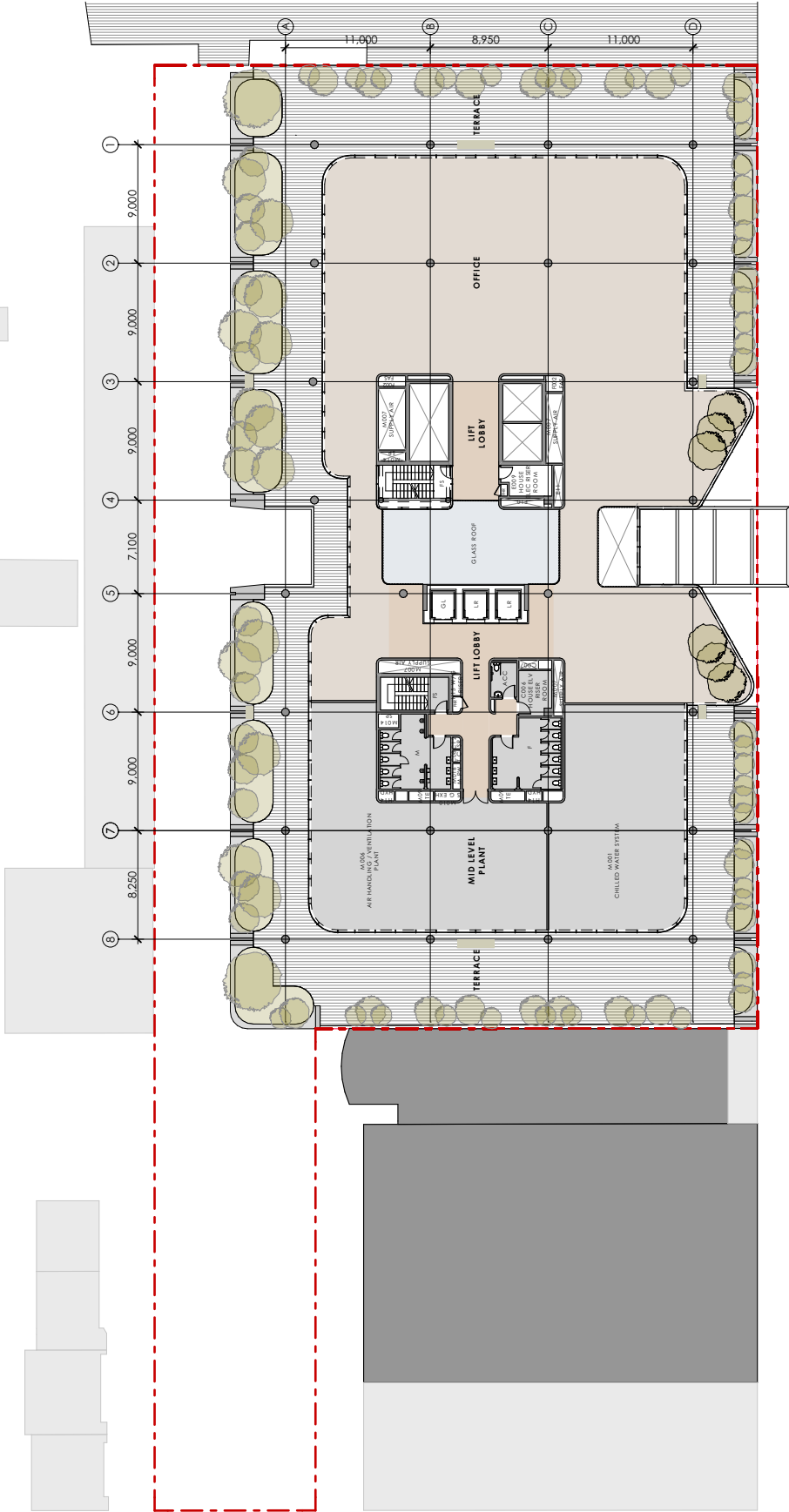


GROUND  
Main access of Pacific Highway, Lobby with Cafe, Retail, Tenant Space Main Building, GFA: 1,535M<sup>2</sup>,  
Parking and Loading Entry of Bruce Street

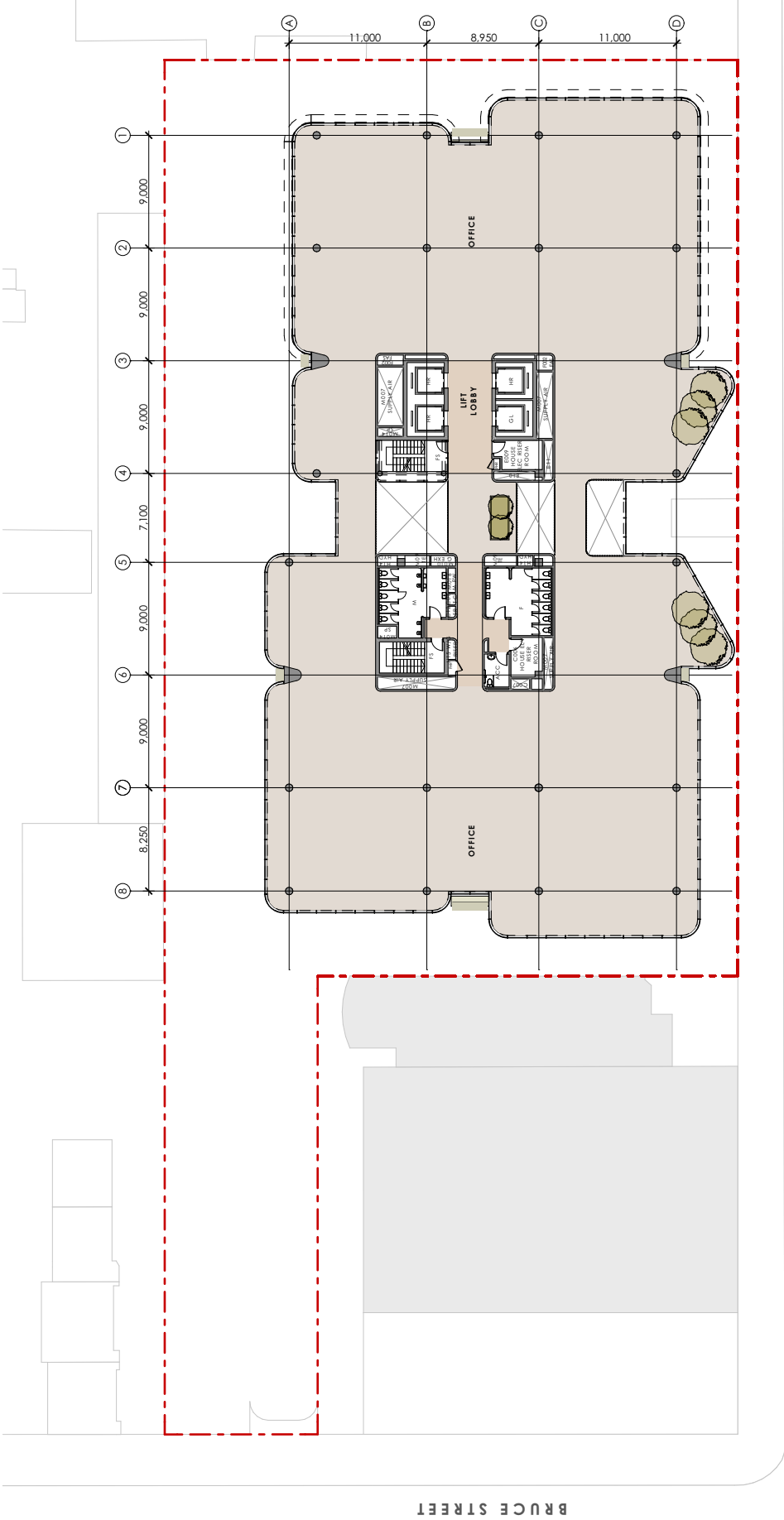




TYPICAL PODIUM FLOOR (L1)  
Tenant Space Main Building, GFA 2,165 m<sup>2</sup>

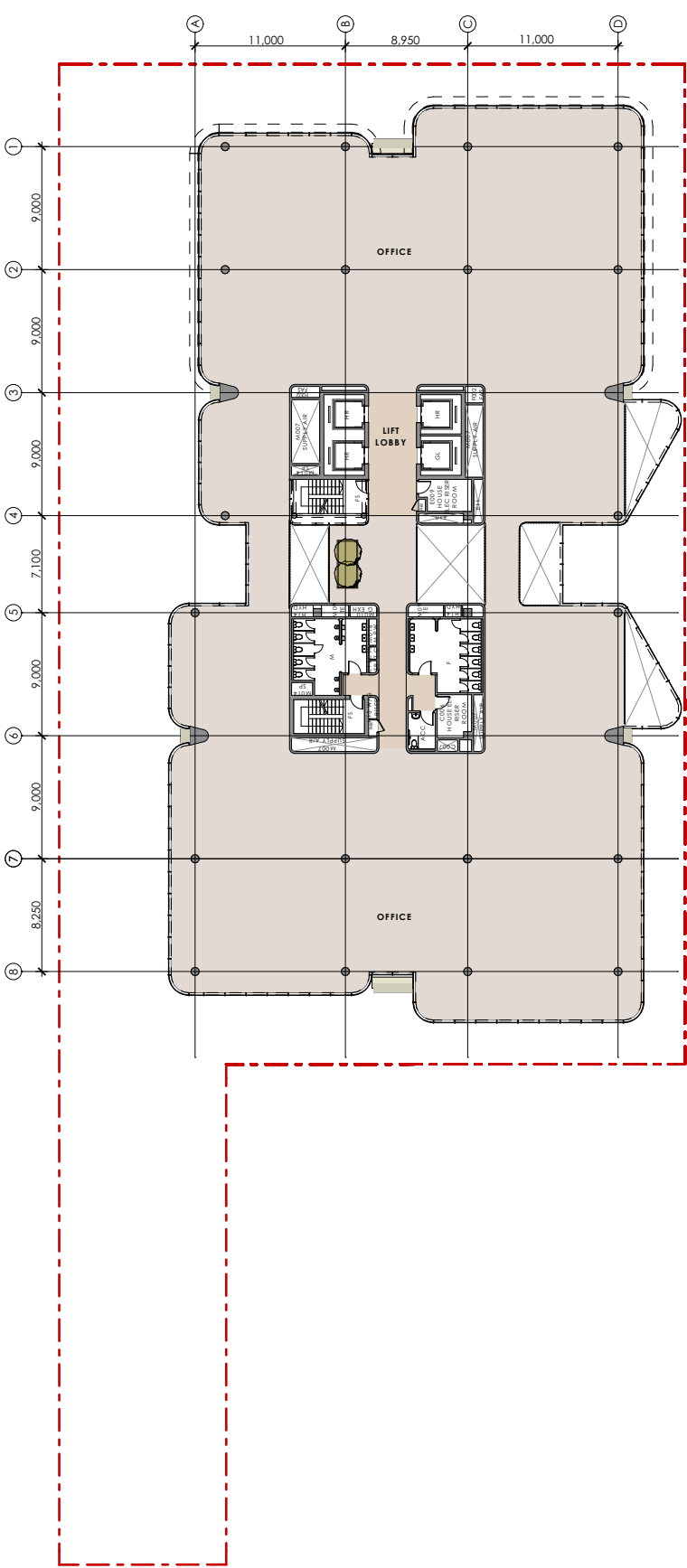


LOW PODIUM ROOF TOP (L3)  
Tenant Space, GFA 975m<sup>2</sup>



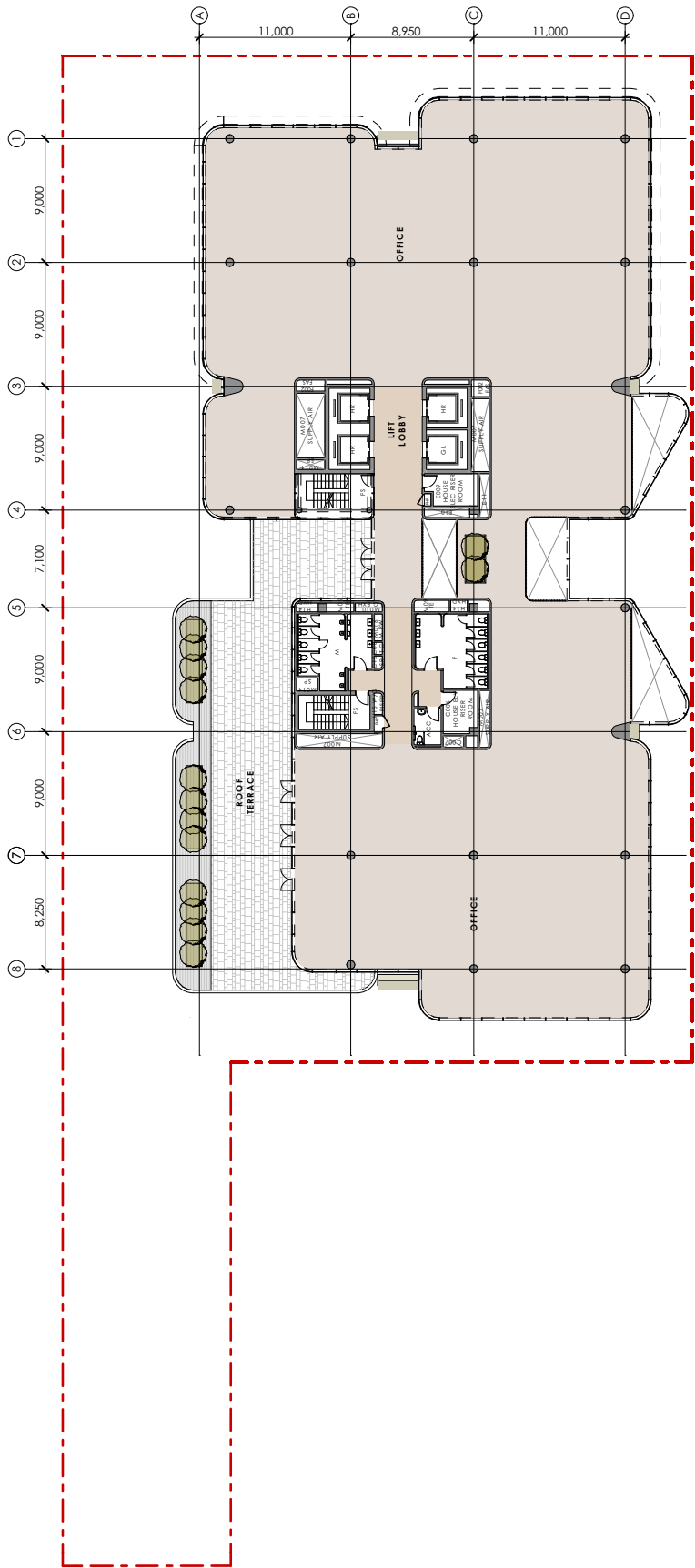
TYPICAL TOWER FLOOR (WITH PLANTER)  
Tenant Space, GFA 1,820m<sup>2</sup>

TYPICAL TOWER FLOOR PLANS

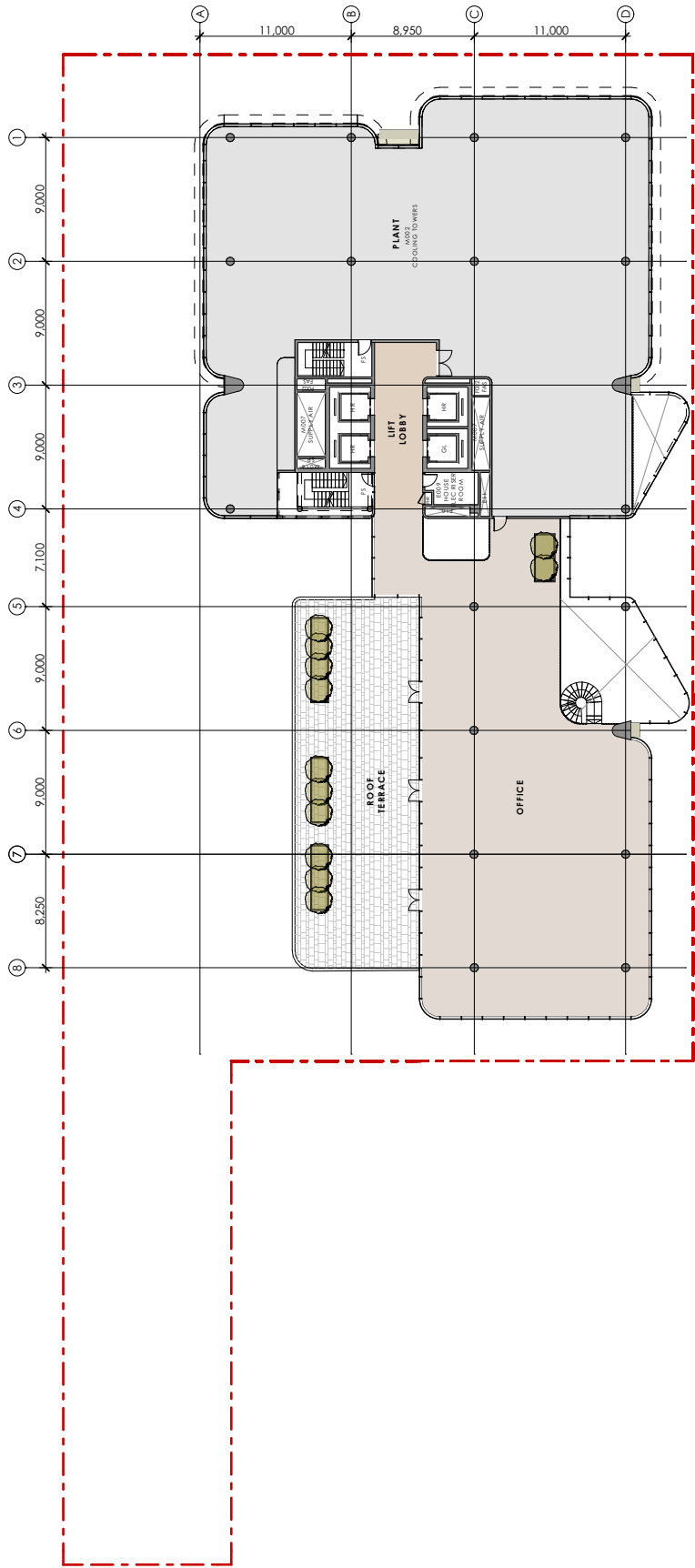


TYPICAL TOWER FLOOR (WITH BALCONY)  
Tenant Space, GFA 1,785 m<sup>2</sup>

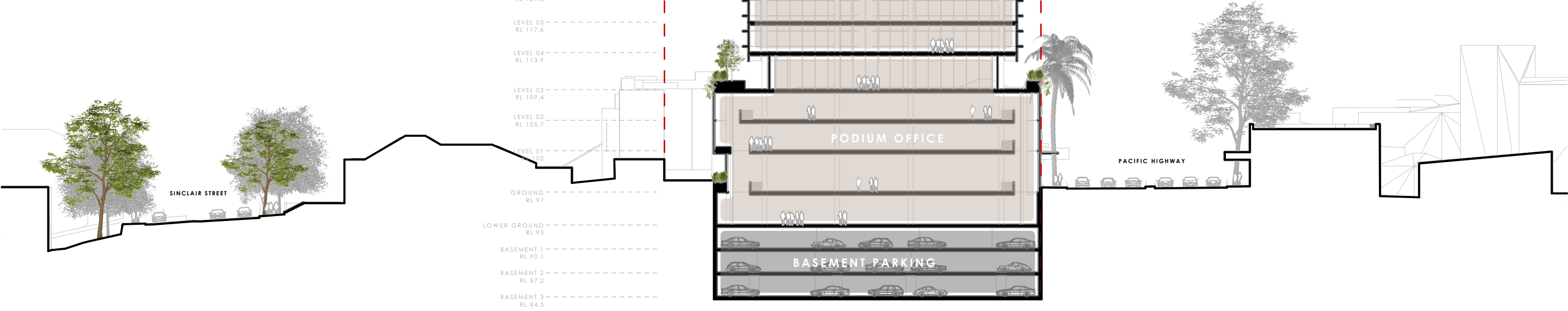
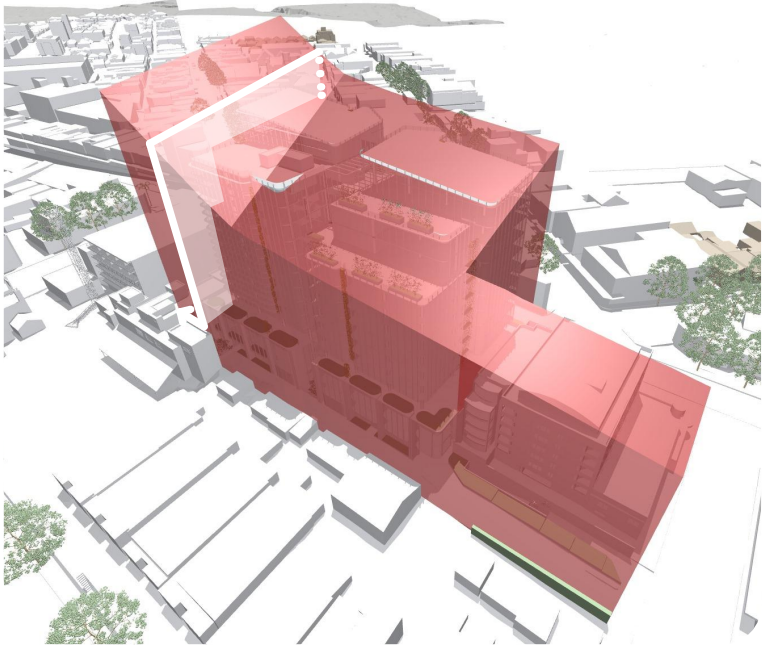




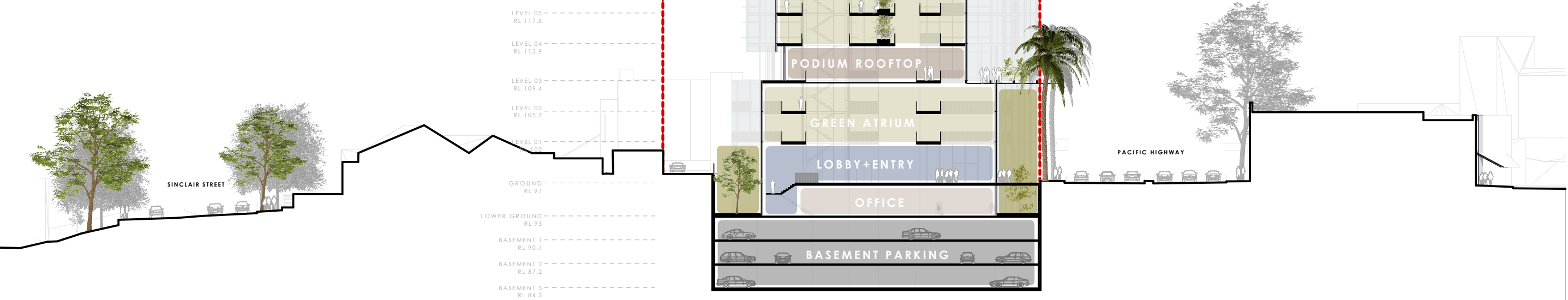
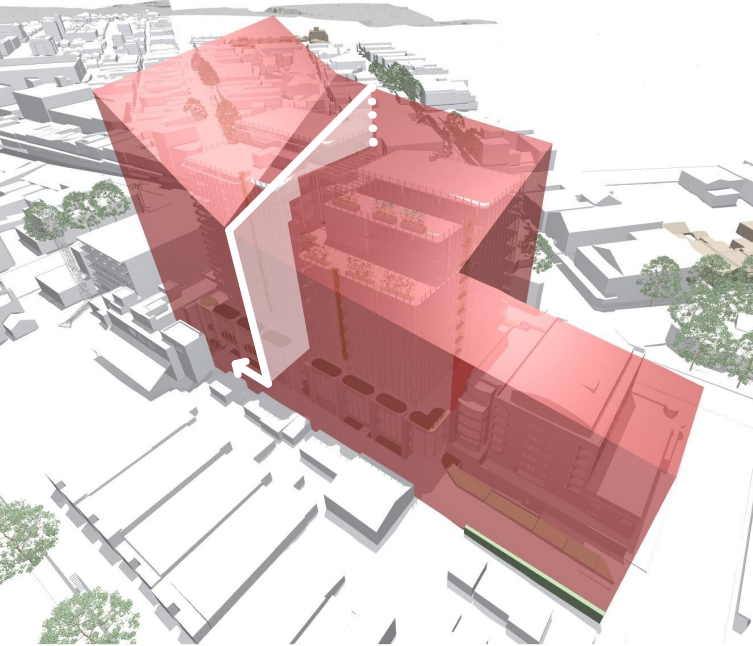
LEVEL 10 (LAST FULL OFFICE FLOOR)  
Tenant Space + Terrace, GFA 1,500 m<sup>2</sup>

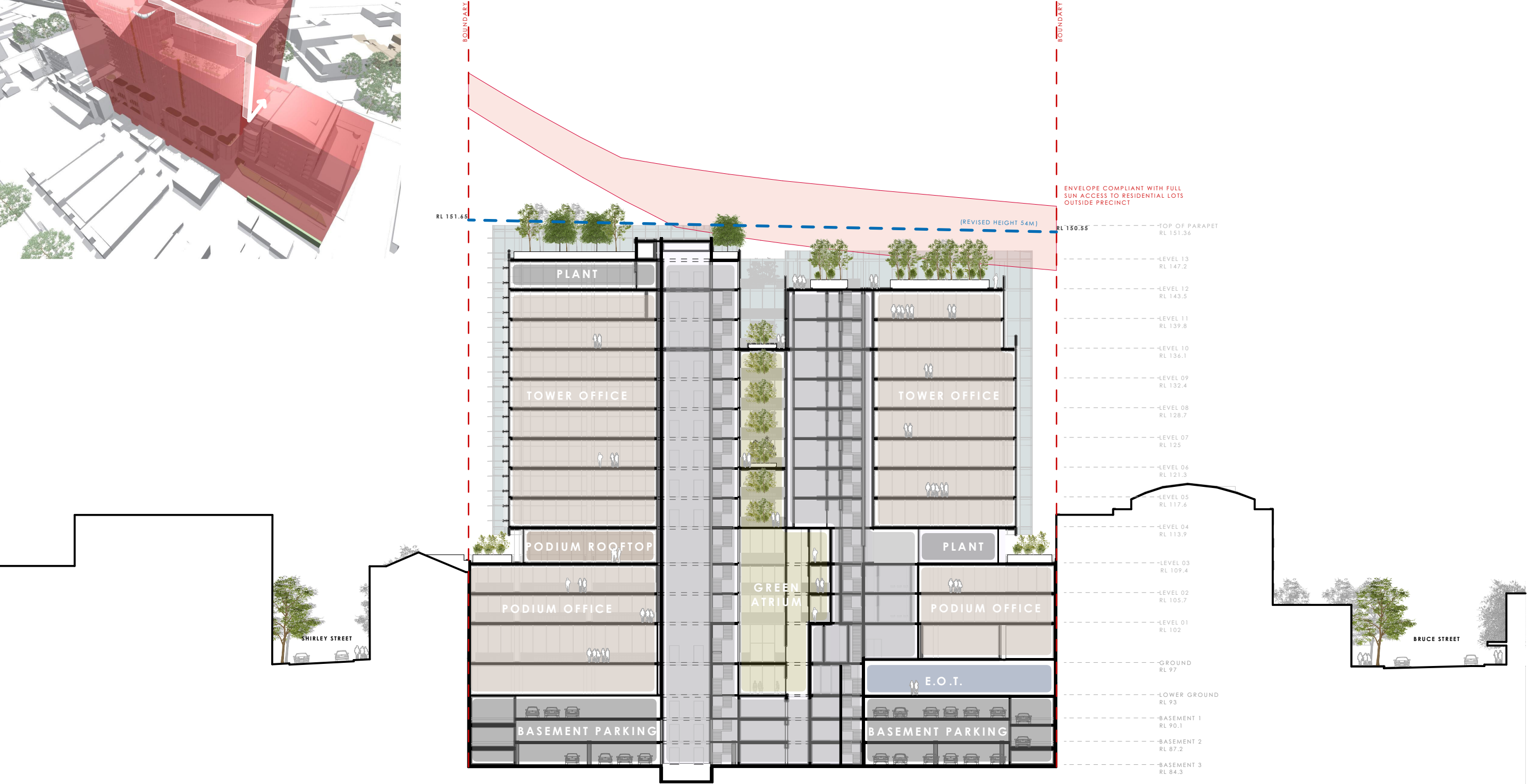
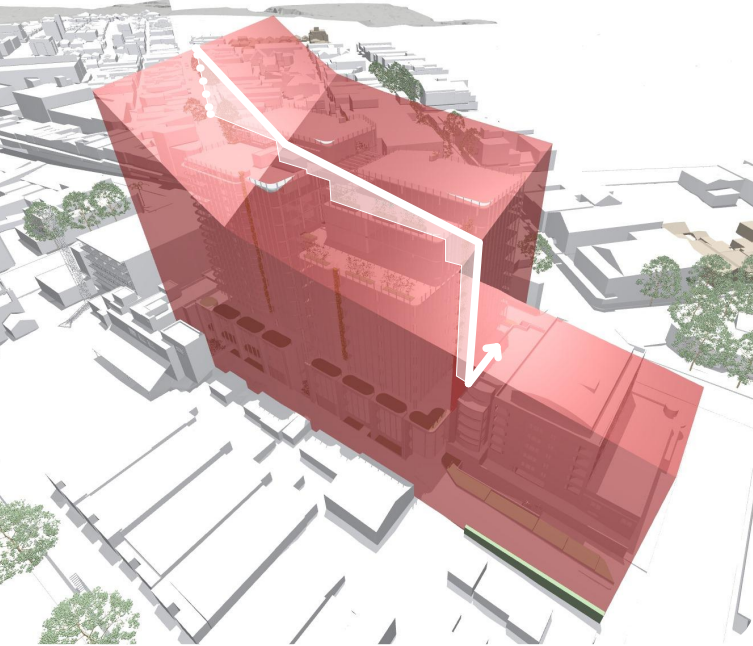


LEVEL 12 (TOP FLOOR)  
Plant, Tenant Space connected to level below + Terrace, GFA 553m<sup>2</sup>

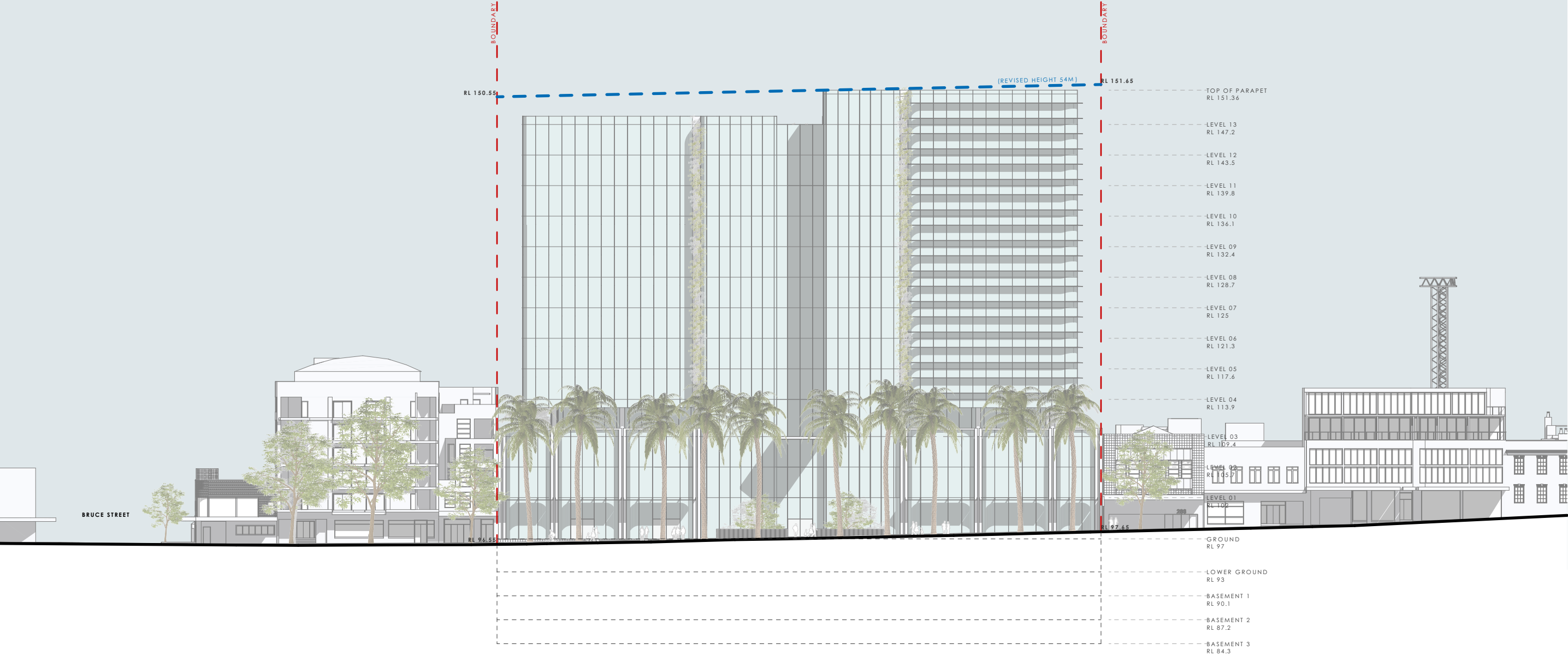


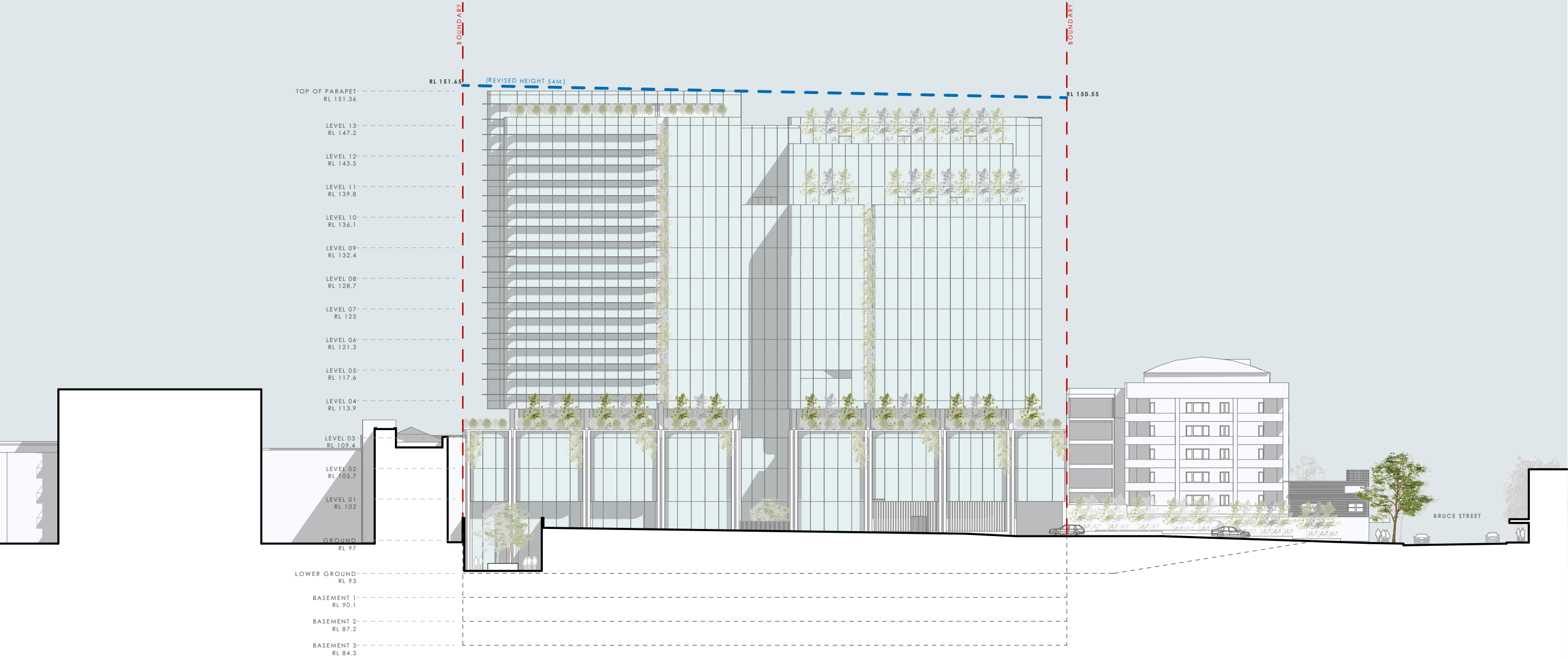




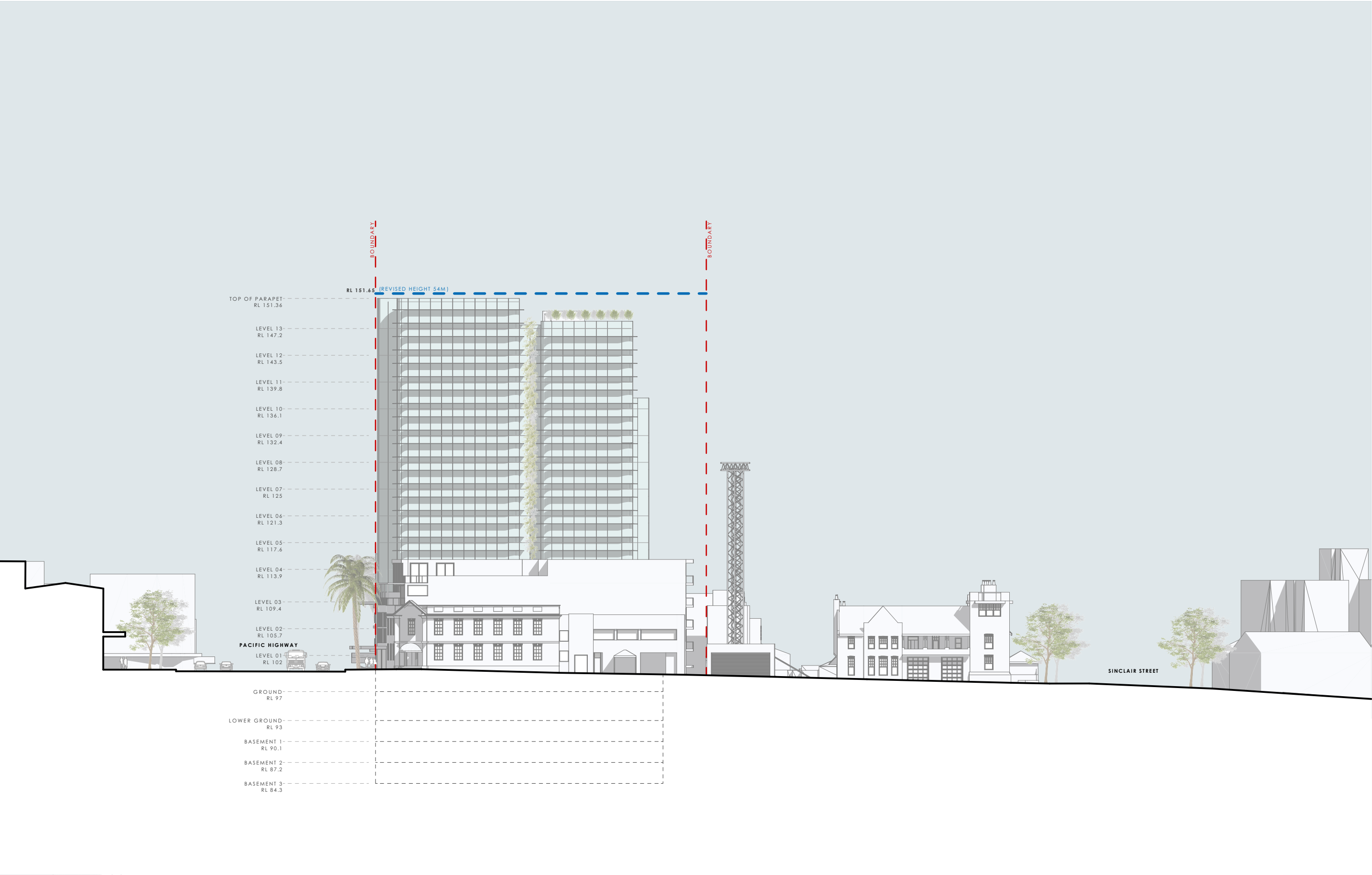


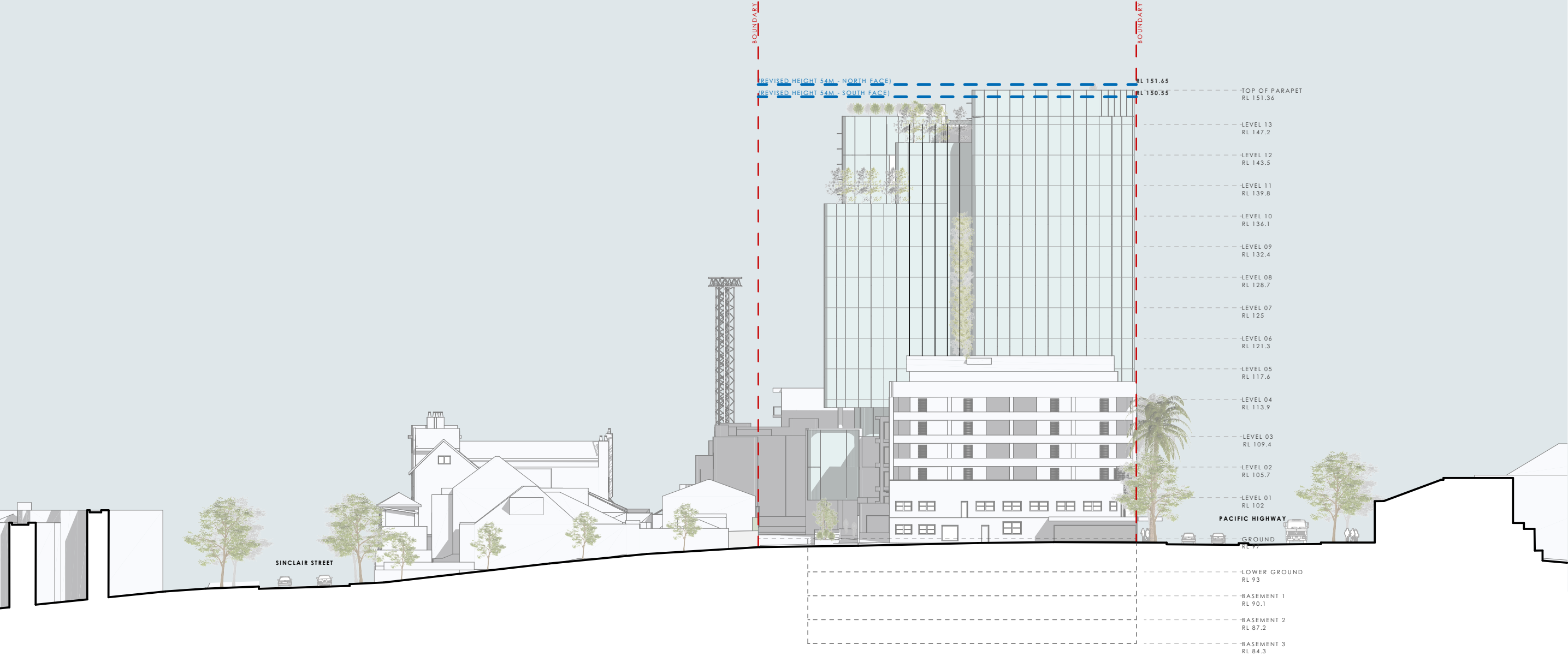






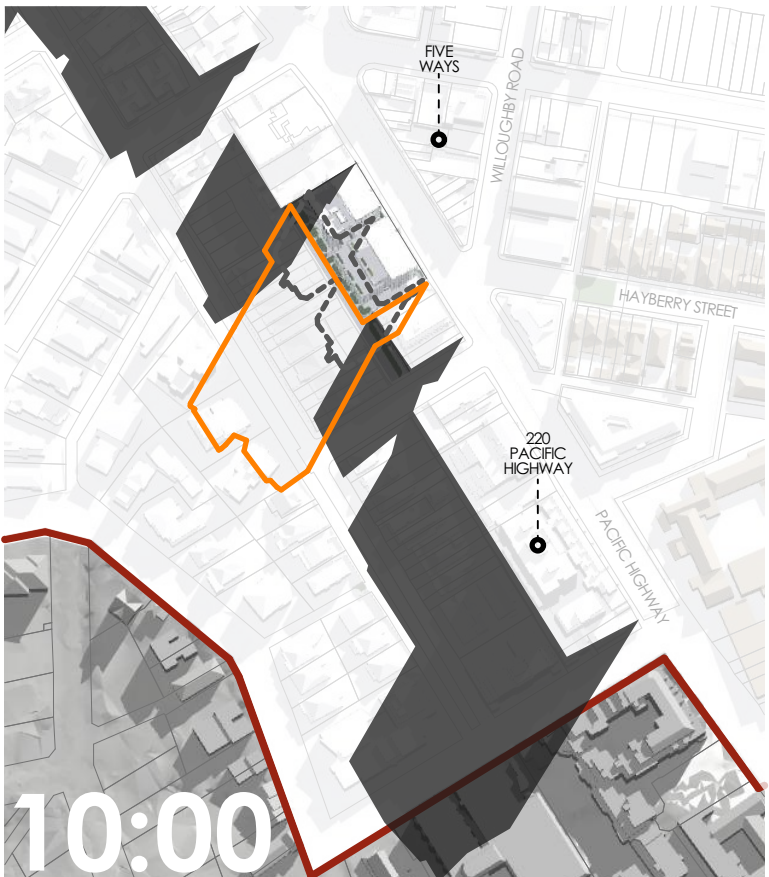
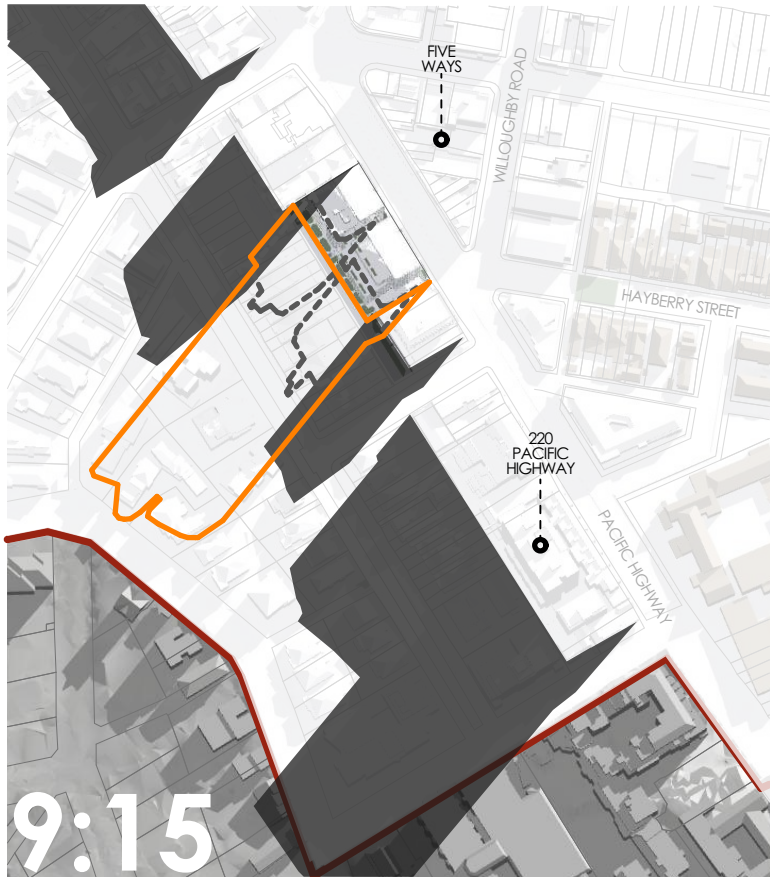
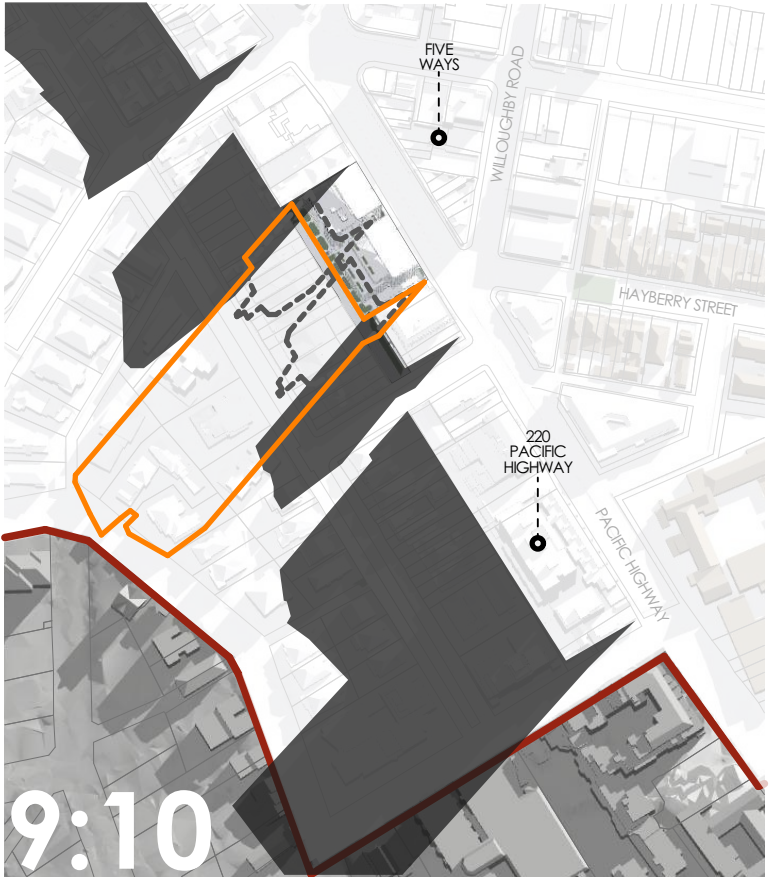










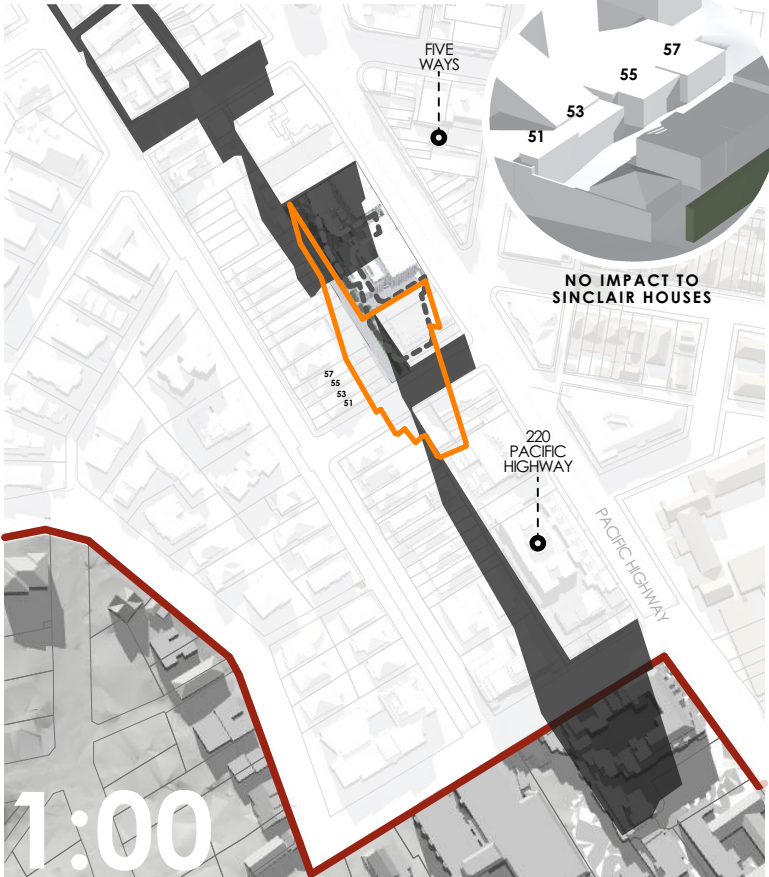
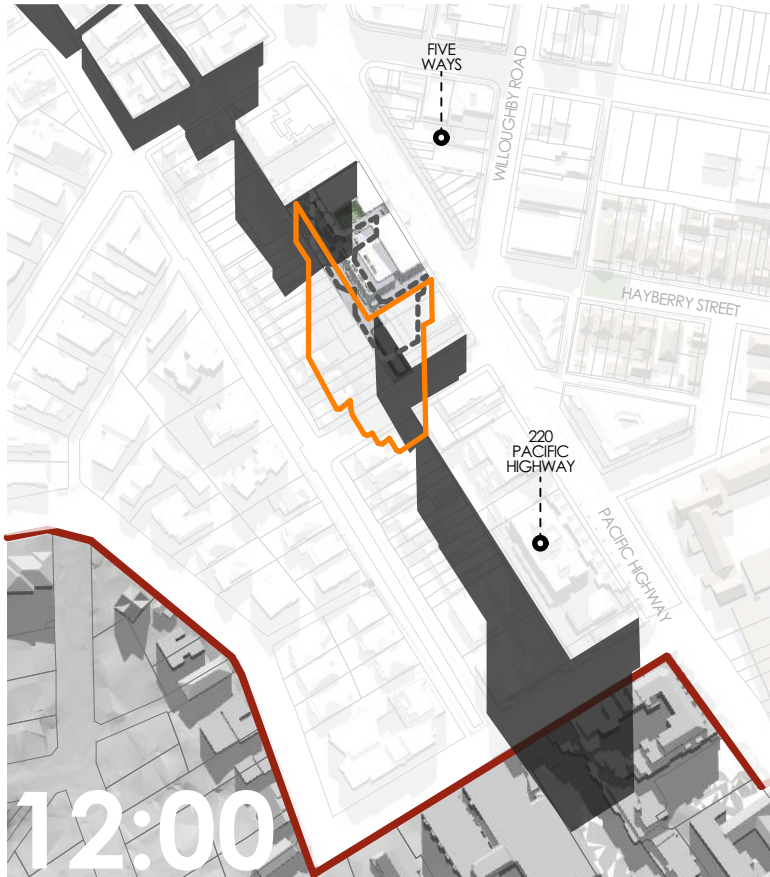
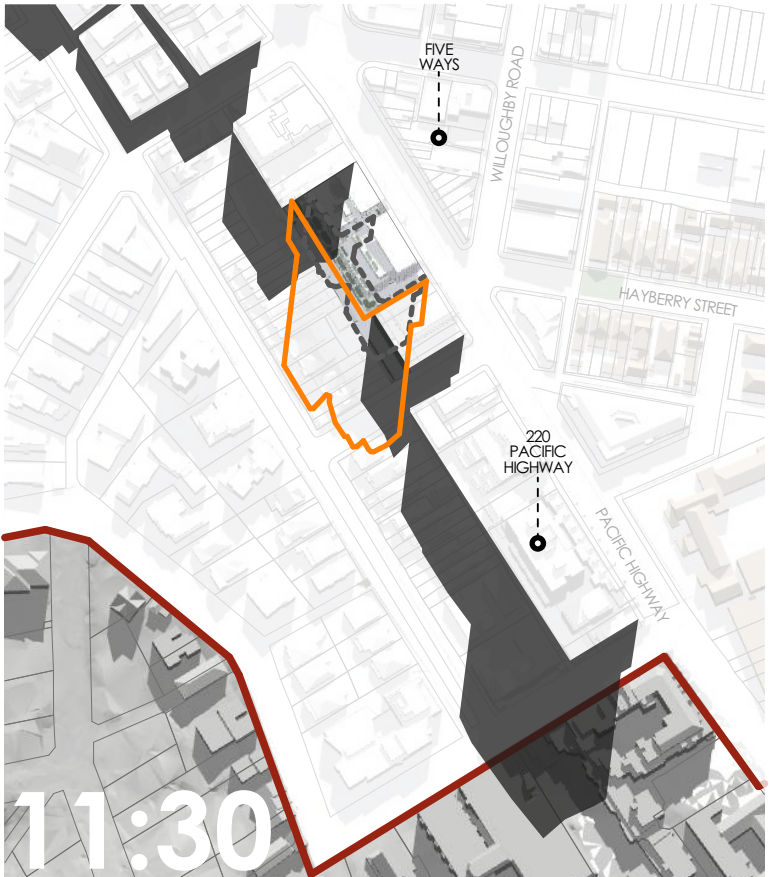
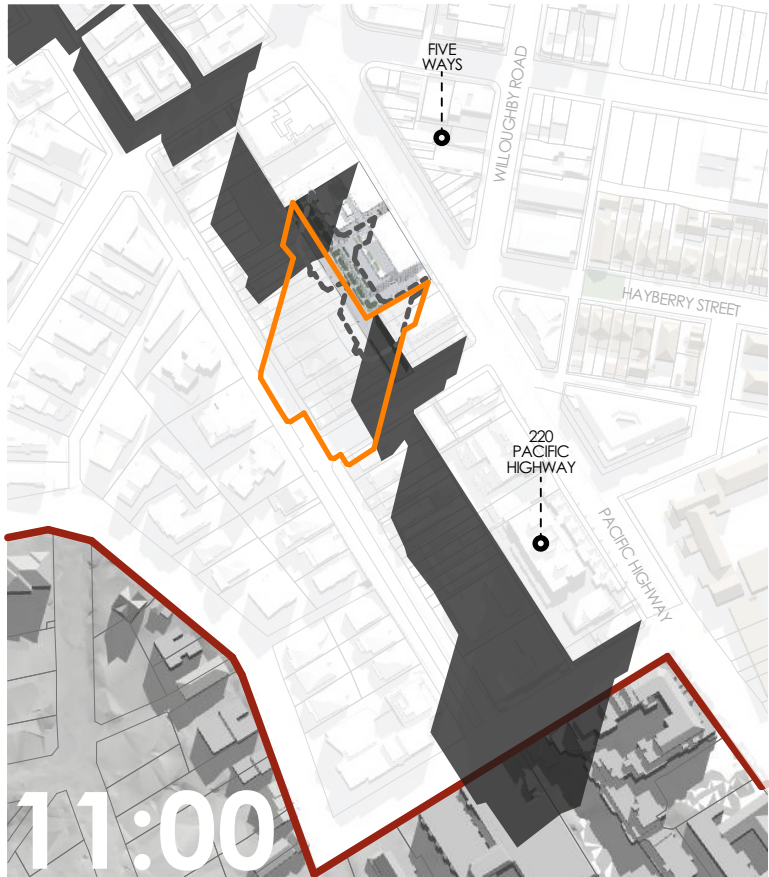
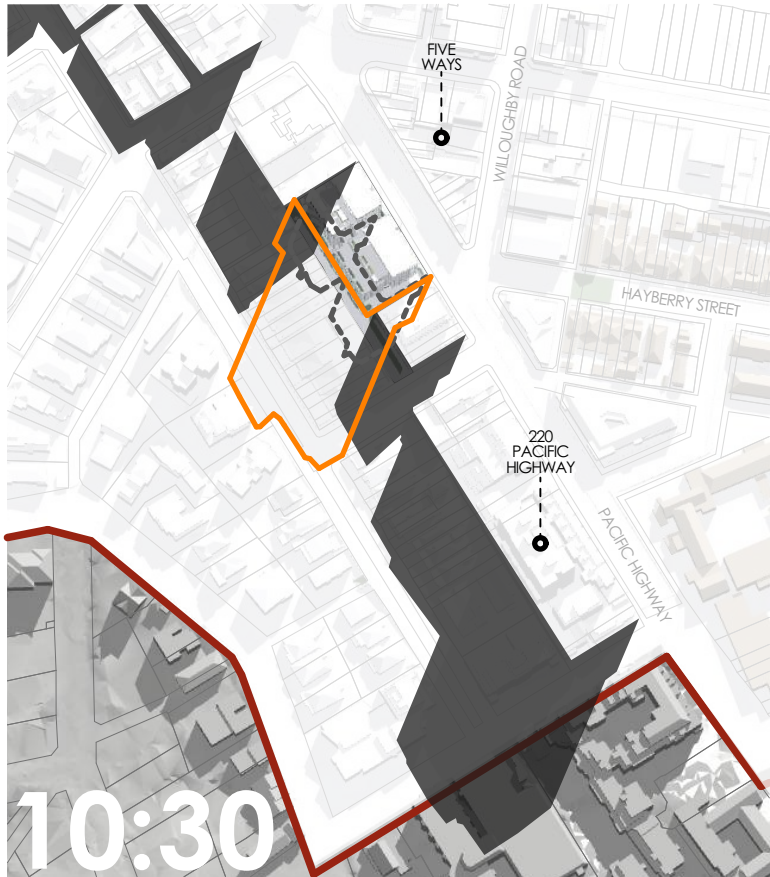


EXISTING BUILDING  
SHADOW 

HEIGHT COMPLIANT  
(13 STOREYS, 6.02 FSR) 

2036 HEIGHTS  
SHADOWS 



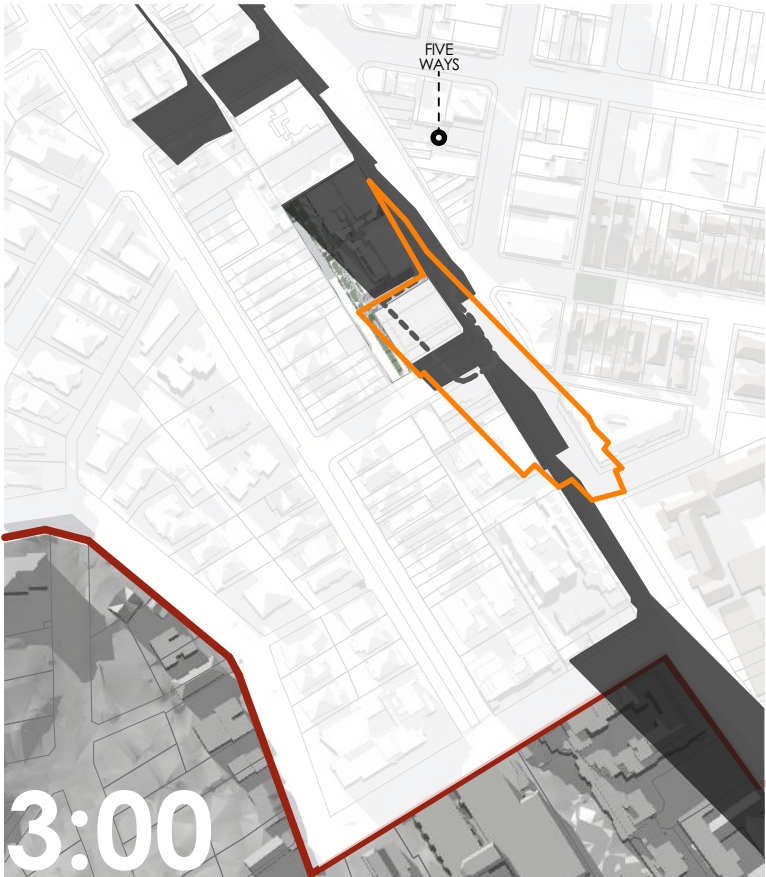
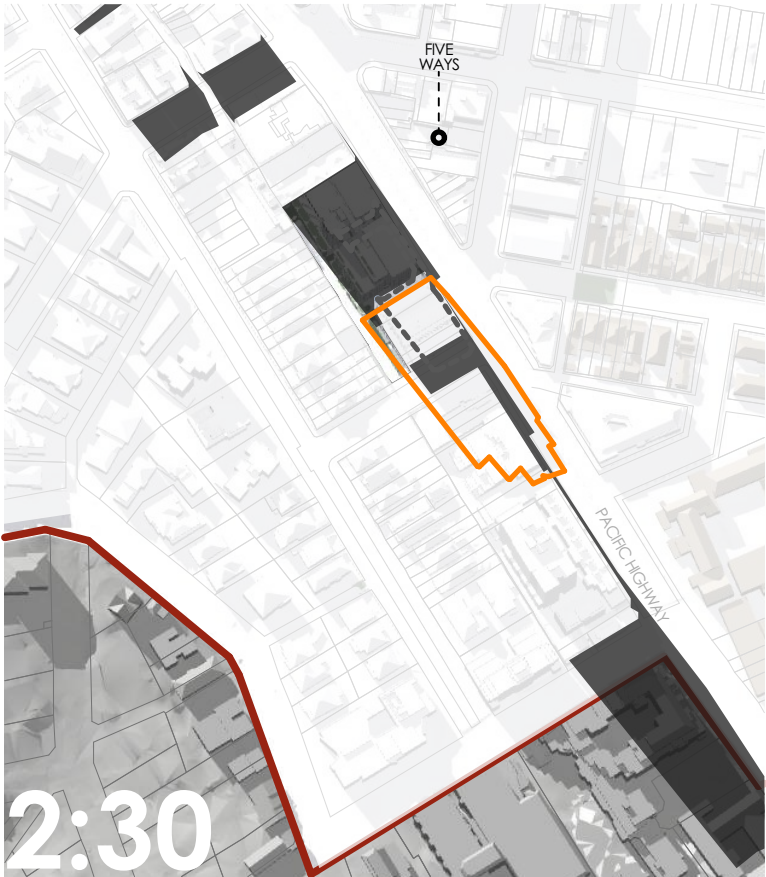
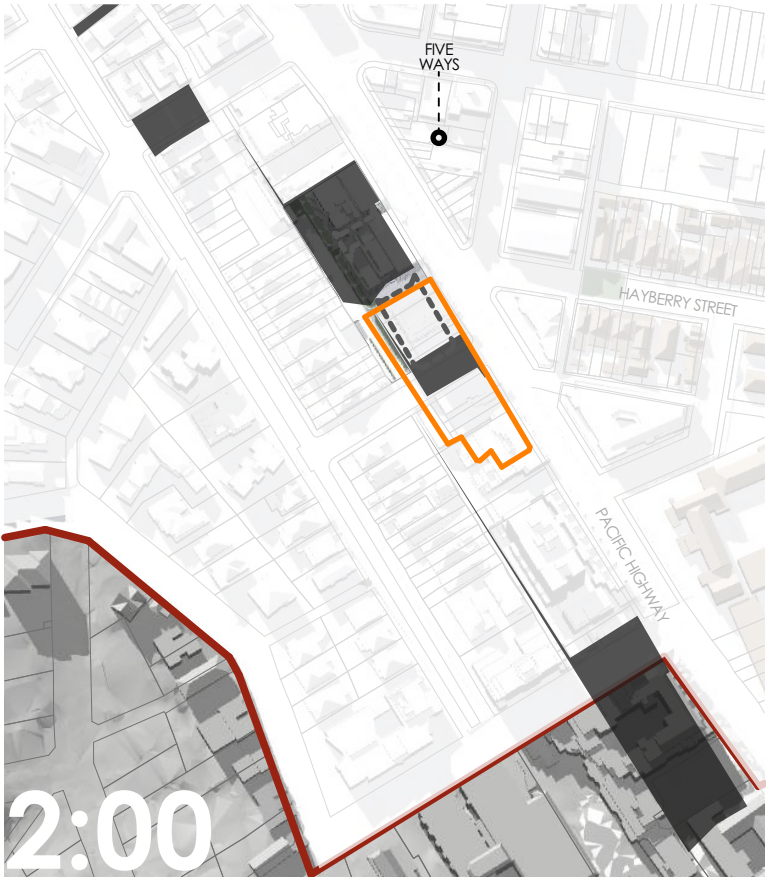
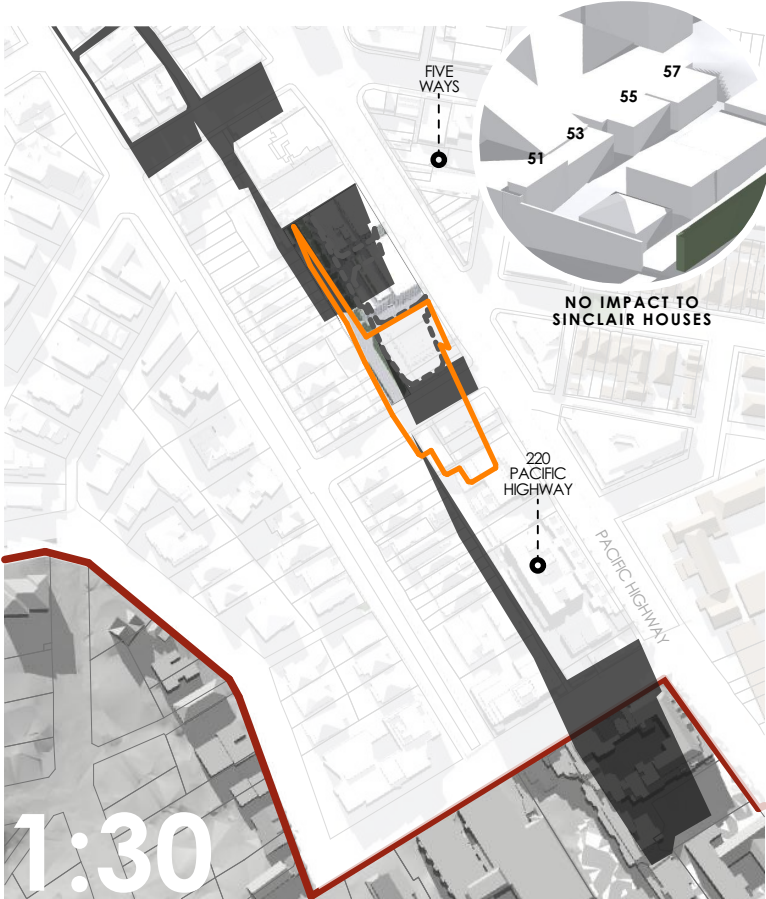


EXISTING BUILDING  
SHADOW ○

HEIGHT COMPLIANT  
(13 STOREYS, 6.02 FSR) ○

2036 HEIGHTS  
SHADOWS ●





EXISTING BUILDING SHADOW 

HEIGHT COMPLIANT (13 STOREYS, 6.02 FSR) 

2036 HEIGHTS SHADOWS 







VIEW 01 - PACIFIC HIGHWAY NORTH



VIEW 02 - HAYBERRY STREET





VIEW 03 - PACIFIC HIGHWAY (SOUTH)



VIEW 04 - WILLOUGHBY ROAD





VIEW 05 - CORNER OF SHIRLEY RD & SINCLAIR ST



VIEW 06 - CORNER OF SINCLAIR & BRUCE STREETS





UPDATED SCHEME  
(13 STOREYS, NO OVERSHADOWING AT ALL TO OUTSIDE BOUNDARY)

Site Area	3,796m²
Allowable FSR	5.60
Proposed FSR Excl. LG GFA	5.60:1
Proposed FSR Incl. LG GFA	6.02:1
Allowable GFA	21,258m²
Proposed GFA Above Ground	21,258m²
Proposed GFA Below Ground	1,595m²
Proposed Total GFA	22,853m²
Allowable Height	13 Storeys
Proposed Height	13 Storeys
Total height above ground	54.00m

Level	Use	RL (m)	Floor-to-Floor (m)	GBA (sqm)	GFA* (sqm)	NLA* (sqm)	GBA/GFA (%)	GFA/NLA (%)
roof	Roof Parapet	151.36						
Level 12	Commerical / Plant	143.50	6.00	1,575	553	470	35%	85%
Level 11	Commercial	139.80	3.70	1,790	1,520	1,290	85%	85%
Level 10	Commercial	136.10	3.70	1,765	1,500	1,275	85%	85%
Level 9	Commercial	132.40	3.70	2,140	1,820	1,550	85%	85%
Level 8	Commercial	128.70	3.70	2,100	1,785	1,515	85%	85%
Level 7	Commercial	125.00	3.70	2,140	1,820	1,550	85%	85%
Level 6	Commercial	121.30	3.70	2,100	1,785	1,515	85%	85%
Level 5	Commercial	117.60	3.70	2,140	1,820	1,550	85%	85%
Level 4	Commercial	113.90	3.70	2,100	1,785	1,515	85%	85%
Level 3	Commercial / Plant	109.40	4.50	1,755	975	745	56%	76%
Level 2	Commercial	105.70	3.70	2,580	2,195	1,865	85%	85%
Level 1	Commercial	102.00	3.70	2,550	2,165	1,840	85%	85%
Ground	Lobby / Commercial / Loading	97.00	5.00	2,725	1,535	940	56%	61%
Lower Ground	EOT / PARKING	93.00	4.00	3,000	1,595	1,355	53%	85%
Basement 1	Parking	90.10	2.90	3,000				
Basement 2	Parking	87.20	2.90	3,000				
Basement 3	Parking	84.30	2.90	1,500				
202 spaces			54m	37,960m²	21,258m²	17,620m²	56%	83%
					22,853m²	18,975m²		

**Notes**

1) Area schedule is a draft and subject to review and update with design development  
2) GBA for basement levels not included in the overall calculations

**Definitions**

<b>Gross Floor Area</b>	<b>Net Lettable Area</b>
GFA is typically defined by the relevant Planning Authority for a project.	NLA as defined by PCA Method of Measurement 2008.
GFA is measured from the internal face of external walls, or from the internal face of walls separating the building from any other building, measured at a height of 1.4 metres above the floor.	NLA means the sum of its whole floor lettable area and is internal walls and internal finished surfaces of dominant portions of the permanent outer building walls.